

Technical Appendix 11.1a

Abnormal Load

Assessment - Southern

Route



This page is intentionally blank.

ABNORMAL LOADS ASSESSMENT



SYSTRA

HAGSHAW ENERGY CLUSTER – WESTERN EXPANSION

ABNORMAL LOADS ASSESSMENT

IDENTIFICATION TABLE

Client/Project owner	3R Energy
Project	Hagshaw Energy Cluster – Western Expansion
Study	Abnormal Loads Assessment
Type of document	Report
Date	17/03/2025
File name	Hagshaw Energy Cluster - Continuous SPA - 100325.docx
Reference number	GB01T24G63/2001/ALA-South
Number of pages	13

APPROVAL

Version	Name		Position	Date	Modifications
1	Author	A Spence	Senior Consultant	19/12/2023	V1
	Checked	A DeVenny	Projects Director	19/12/2023	
	Approved	A DeVenny	Projects Director	19/12/2023	
2	Author	A Spence	Senior Consultant	07/02/2024	V2
	Checked	A DeVenny	Projects Director	07/02/2024	
	Approved	A DeVenny	Projects Director	07/02/2024	
3	Author	T Foster	Consultant	10/03/2025	Updated ALA route
	Checked	A DeVenny	Projects Director	10/03/2025	
	Approved	A DeVenny	Projects Director	10/03/2025	
4	Author	T Foster	Consultant	17/03/2025	Minor changes
	Checked	A DeVenny	Projects Director	17/03/2025	
	Approved	A DeVenny	Projects Director	17/03/2025	

TABLE OF CONTENTS

1.	INTRODUCTION	5
1.1	GENERAL	5
1.2	TURBINE TRANSPORT REQUIREMENTS	7
1.3	SWEPT PATHS	8
2.	ASSESSMENT OUTCOMES AND CONCLUSIONS	11
2.1	HORIZONTAL FEASIBILITY	11
2.2	SUMMARY	11

LIST OF FIGURES

Figure 1.	Site Location	5
Figure 2.	Proposed Southern Abnormal Loads Route	6
Figure 3.	Turbine Delivery Vehicle	7

LIST OF TABLES

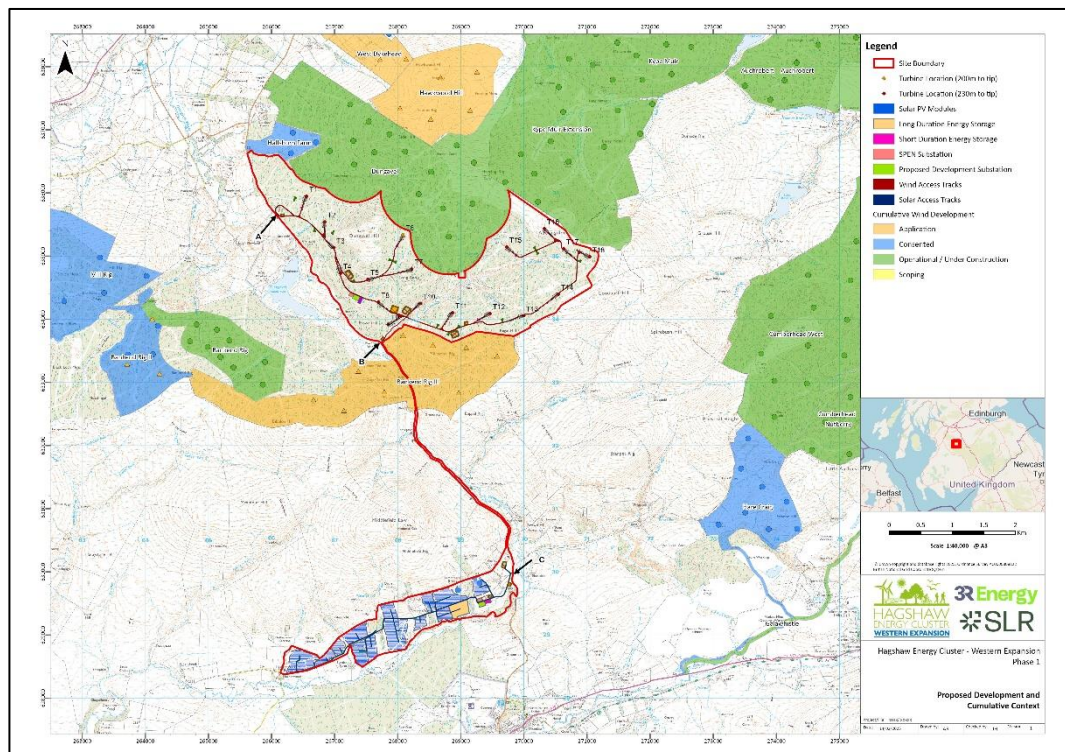
Table 1.	Pinch Point Summary – Blade Delivery Vehicle	9
----------	--	---

1. INTRODUCTION

1.1 General

1.1.1 SYSTRA Ltd (SYSTRA) has been commissioned by 3R Energy to undertake an assessment of a potential abnormal loads route to the Hagshaw Energy Cluster Western Expansion, located approximately 6km north of Muirkirk, Ayrshire. The site location is indicated by **Figure 1** below.

Figure 1. Site Location



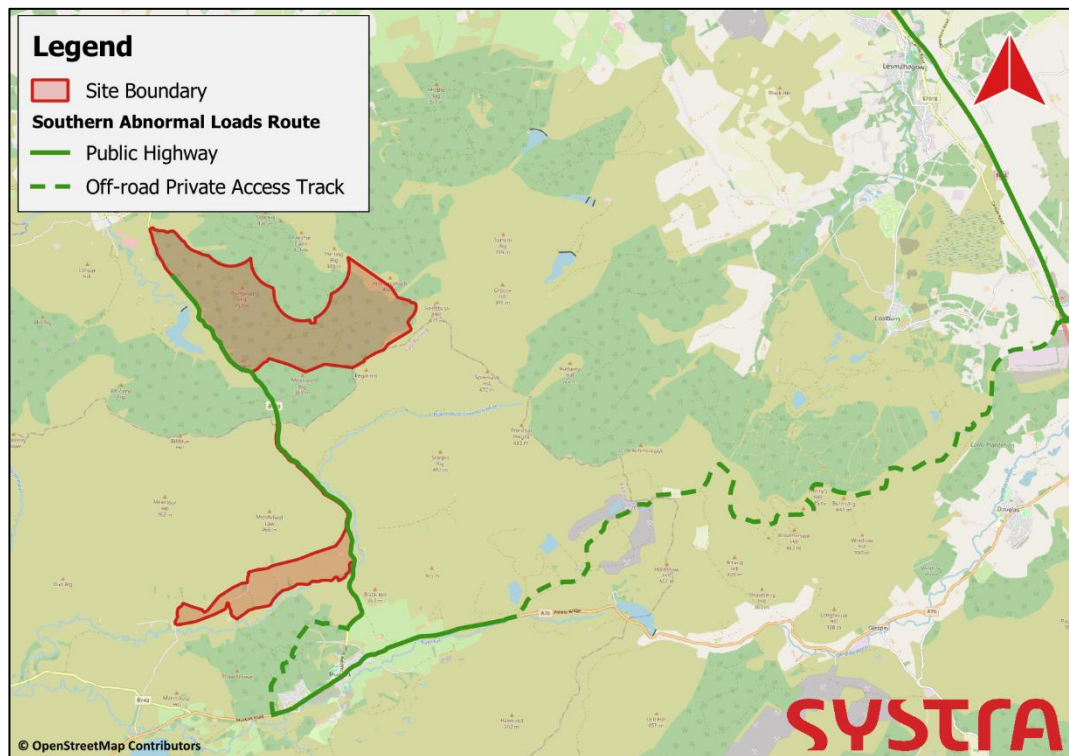
1.1.2 SYSTRA has previously undertaken an Abnormal Loads Assessment (ALA) for the proposed wind farm, arriving at the site from the north. The purpose of this document is to explore an alternative route into the Hagshaw Energy Cluster Western Extension from the south.

1.1.3 This assessment report assesses the route outlined below:

- Via Junction 11 of the M74, through Douglas West / Hagshaw Hill / Galawhistle wind farms and Spireslack to meet with the A70 at the old opencast entrance – which is used for the consented but not constructed Hare Craig wind farm – going through Muirkirk then turning north into Forestry and Land Scotland (FLS) land. North of Muirkirk the route joins the B743 heading north to site access point A.

1.1.4 **Figure 2** below demonstrates the route to site from the M74 motorway through existing wind farms from the east, around Muirkirk and north on the B743 to site access point A.

Figure 2. Proposed Southern Abnormal Loads Route

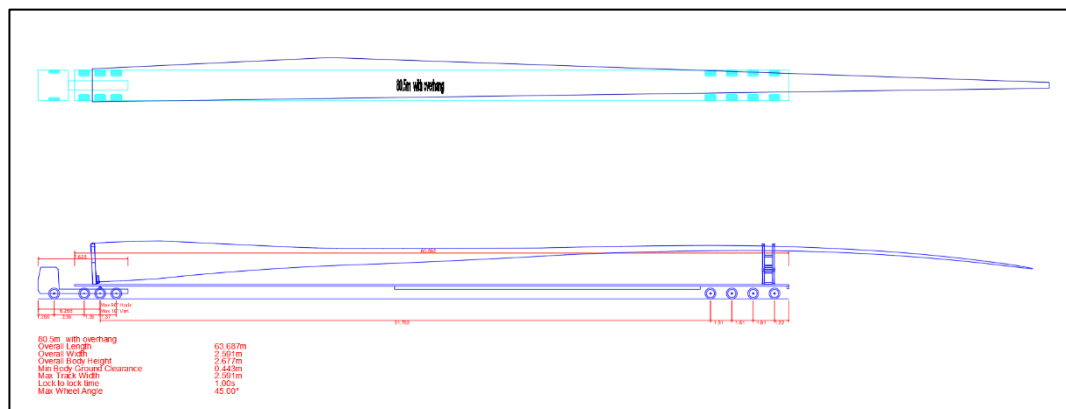


1.1.5 This ALA identifies pinch points, which are points of interest, i.e. bends, junctions etc that will need to be overcome in order to facilitate the safe and efficient movement of abnormal load vehicles. Swept path assessments have been undertaken based on the movement of the blade components to assess whether the route is feasible and to establish the level of mitigation that may be required on the route.

1.2 Turbine Transport Requirements

- 1.2.1 The candidate turbine for the Hagshaw Energy Cluster – Western Expansion has not yet been selection, but it is anticipated that the blades to be transported will be approximately 80.5m in length.
- 1.2.2 The turbine blades would be loaded onto a specialised blade trailer. For the purposes of assessment, it has been assumed that the Turbine Delivery Vehicle (TDV) would compose of a tractor unit and a “super wing” blade carrier.
- 1.2.3 The super wing carrier incorporates pendle axles which allow for a secondary rear wheel steering angle of up to 60 degrees. The super wing trailer can be extended to 64.3m in length. For this assessment it has been assumed that the trailer would be retracted by approximately 4m for a length of 60.5m. This assumption is based on the loading configuration of other wind turbine blade components. In total the TDV is ~63.6m in length. The turbine blade would overhang the end of the trailer by ~22m.
- 1.2.4 A drawing of the TDV is provided in **Appendix A** and at **Figure 2** below.

Figure 3. Turbine Delivery Vehicle



- 1.2.5 At this stage in the design process, the haulier of the wind turbine components is not yet known, nor the exact specifics of the vehicles that they will use. However, from experience of previous developments, and from the details of the components to be moved, it is possible to estimate (with some degree of accuracy) the specifics of the design vehicles that will be used to allow a detailed and accurate route assessment to be undertaken.
- 1.2.6 SYSTRA has used the Autodesk (CAD) extension package AutoTrack to undertake swept path analysis (SPA) for the identified Pinch Points (PP) on the delivery route. The trailer units associated with the design vehicle for the transportation of the blades are extendable so on delivery of the abnormal load, the trailer can be reduced in length. In these circumstances, the AutoTracking and route assessment has only been undertaken for the delivery route to the site.

1.3 Swept Paths

- 1.3.1 The swept path plans are contained within **Appendix A**. The plans indicate areas of overrun and over-sail, as well as highlighting potential clashes with street furniture and requirements for third-party land.
- 1.3.2 **Table 2** summarises the findings at each location for the transportation of the blade component.

Table 1. Pinch Point Summary – Blade Delivery Vehicle

SPA	LOCATION	STREET FURNITURE REMOVAL	TEMPORARY PAVING / GRADING WORKS	3RD PARTY LAND REQUIREMENTS
01	M74 Junction 11 Southbound Exit (Turn from public road onto private access tracks)	<ul style="list-style-type: none"> 3no. chevron signs 2no. traffic signs 2no. lighting columns 1no. bollard 	<ul style="list-style-type: none"> Hardstanding overrun areas required in roundabout islands 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required
02	Exit from Private Access Tracks onto A70	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None anticipated – lands likely under option
03 – 10	A70 Between open cast mine and east of Muirkirk	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None
11	“S”-bend east of Muirkirk	<ul style="list-style-type: none"> Telephone line to be grounded 	<ul style="list-style-type: none"> Transition ramp may be required to enable TDV to mount kerb 	<ul style="list-style-type: none"> None
12 – 13	A70 through Muirkirk	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None
14	Right-hand turn into FLS land (turn-off to join private access route)	<ul style="list-style-type: none"> 2no. lighting column 1no. road sign 	<ul style="list-style-type: none"> Hardstanding overrun area required to make turn 	<ul style="list-style-type: none"> Yes – Overrun and oversail into third party land. Land checks required
15	Left-hand turn onto B743 (turn from private access track back onto public road)	<ul style="list-style-type: none"> 2no utility columns and lines 	<ul style="list-style-type: none"> Hardstanding overrun area required opposite turn to elevation change on inside of turn 	<ul style="list-style-type: none"> Yes – Overrun and oversail into third party land. Land checks required
16	Continuous left-hand bend in B743 north of Muirkirk	<ul style="list-style-type: none"> Utility columns and lines Potential conflict with mature trees 	<ul style="list-style-type: none"> Hardstanding required on outside of bend 	<ul style="list-style-type: none"> Yes – Overrun and oversail into third party land. Land checks required
17 – 19	B743 north of Muirkirk	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None
20	B743 north of large culvert	<ul style="list-style-type: none"> Telephone line to be grounded 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None
21	Bend in B743 south of farm access crossroads	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required
22	Bends in B743 north of farm access crossroads	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required
23	Long continuous bend in B743 north of cattery	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required
24	Left-hand bend in B743 at farm access	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required
25 - 26	B743 north of farm access	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None
27	B743 at Dippal Lodge access track	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required
28	Series of bends in B743 north of Dippal Lodge access track	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Yes – Over-sail into potential third-party land. Land checks required

SPA	LOCATION	STREET FURNITURE REMOVAL	TEMPORARY PAVING / GRADING WORKS	3RD PARTY LAND REQUIREMENTS
29	Series of bends in B743 south of culvert	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into potential third-party land. Land checks required
30-31	B743 south of forestry	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None
32	B743 at entrance to forestry	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into potential third-party land. Land checks required
33	B743 at forestry crossroad	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into potential third-party land. Land checks required
34	B743 south of break in forestry	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None
35	Left-hand bend in B743 at site access point B	<ul style="list-style-type: none">Vegetation to be trimmed	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into third-party land. Land checks required
36	B743 north of site access point B	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into potential third-party land. Land checks required
37	B743 north of site access point B	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None
38	B743 near layby	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into potential third-party land. Land checks required
39	Series of tight bends in B743	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into third-party land. Land checks required
40	Series of bends in B743 east of reservoir dam	<ul style="list-style-type: none">4no. chevron signs	<ul style="list-style-type: none">None	<ul style="list-style-type: none">Yes – Over-sail into third-party land. Land checks required
41 - 43	B743 north of reservoir	<ul style="list-style-type: none">None	<ul style="list-style-type: none">None	<ul style="list-style-type: none">See site access drawing GB01T2463/2001/I/GL002 for details

2. ASSESSMENT OUTCOMES AND CONCLUSIONS

2.1 Horizontal Feasibility

- 2.1.1 The abnormal load assessment has identified that the route from the M74 to the site is technically feasible in terms of horizontal geometry, which is dependent upon third-party land agreements at a number of locations. It is noted that this report has focussed on the horizontal alignment of the route. No vertical checks have been undertaken at this point, but no major impediments have been identified at this stage and no weight restrictions have been identified on the route at this stage.

2.2 Summary

- 2.2.1 In the first instance, it is recommended that third-party land enquiries are made at the identified locations, and it is also recommended that further engagement is undertaken with the Clyde Port Authority, South Lanarkshire Council, East Ayrshire Council, Transport Scotland and AMEY (the M74 Trunk Road operator).

- 2.2.2 Additionally, the following points should also be considered:

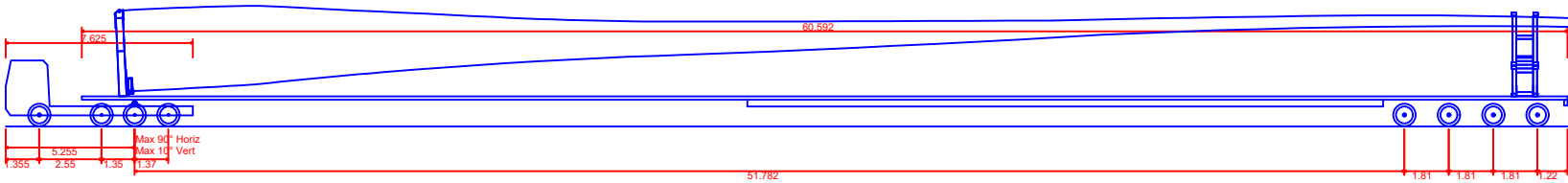
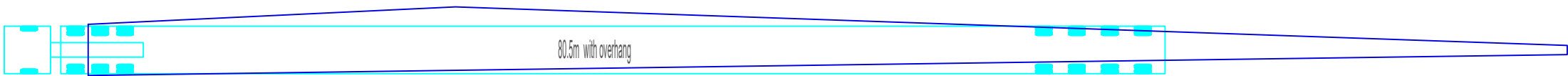
- If the necessary mitigation measures are achievable then the route can be navigated by vehicles carrying wind turbine components to the proposed site subject to confirmation of structural checks;
- Police escort or Pilot car will be required to accompany abnormal loads in order to assist with traffic control and the control of oncoming traffic;
- It is recommended to have adequate warning signs implemented to warn other road users at critical points along the route;
- All hedges, shrubs, bushes, trees and overhanging branches along the nominated routes must be trimmed from within carriageway verges;
- Specific street furniture has been nominated in this report for removal to facilitate over-sailed and 'swept' areas;
- Full carriageway widths must be available along the entirety of the route. Measures to remove parked vehicles must therefore be in place;
- Measures to assist the vehicle negotiating the kerbed changes in level at footways, roundabouts, pedestrian refuges etc. will be required; and
- Steel road plates may be required at locations where the vehicle overruns utility boxes or footways.

Appendix A – TDV Drawing and Swept Path Assessment

- Notes:
- Do not scale from drawing.
 - All dimensions in metres unless otherwise stated.
 - Please note that marked service locations are indicative only and should be confirmed by topographical survey.



- Key:
- Blade Transporter Vehicle
 - Blade Outline



80.5m with overhang
Overall Length 63.687m
Overall Width 2.591m
Overall Body Height 2.677m
Min Body Ground Clearance 0.443m
Max Track Width 2.591m
Lock to lock time 1.00s
Max Wheel Angle 45.00°

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

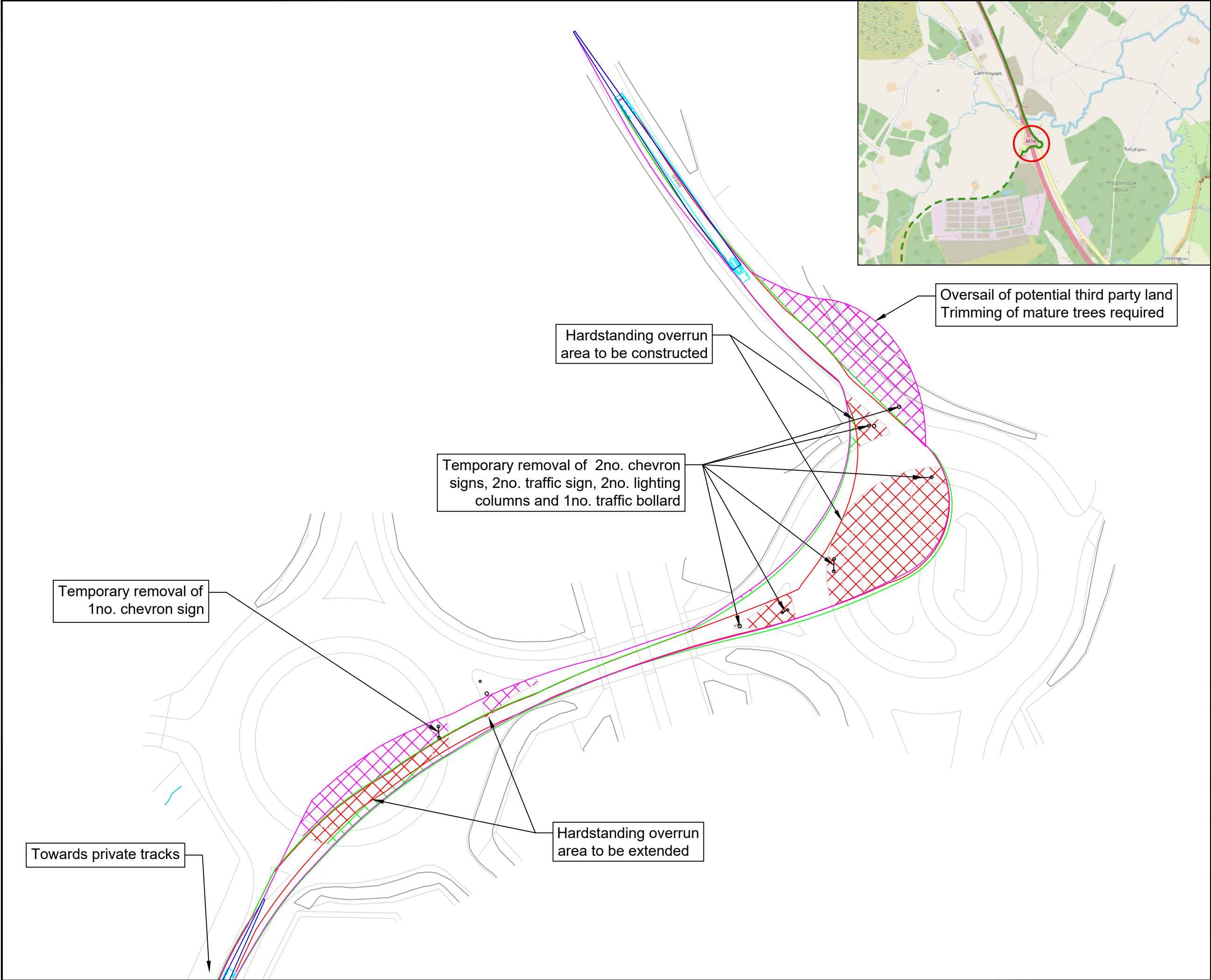
Project

Hagshaw Energy Cluster
Western Expansion

Title

80.5m Blade "Super Wing" Blade Transport
Indicative Loading Configuration

Drawn	TF	Checked	ADV	Approved	ADV
Original dtp. size	A3	Date	17/03/2025	Scale	NTS
Drawing Status	Information	Drawing Number	GB01T24G63/2001/II/SA/VEH001	Rev.	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

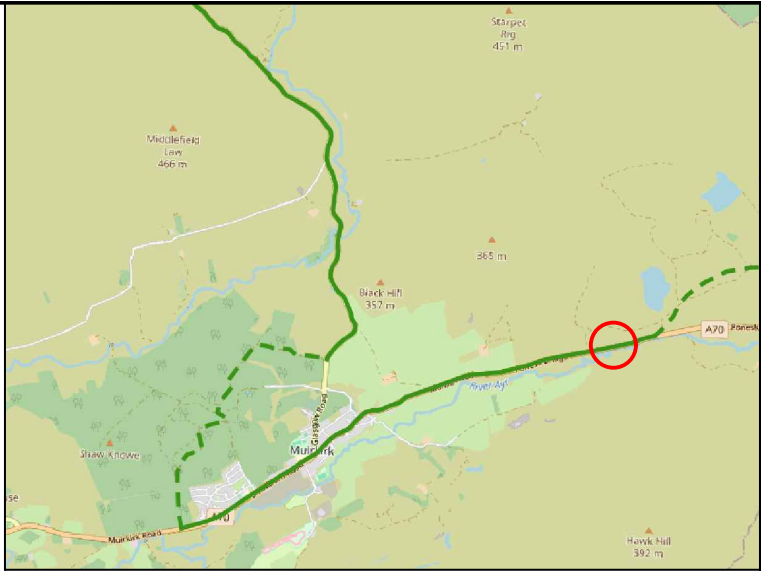
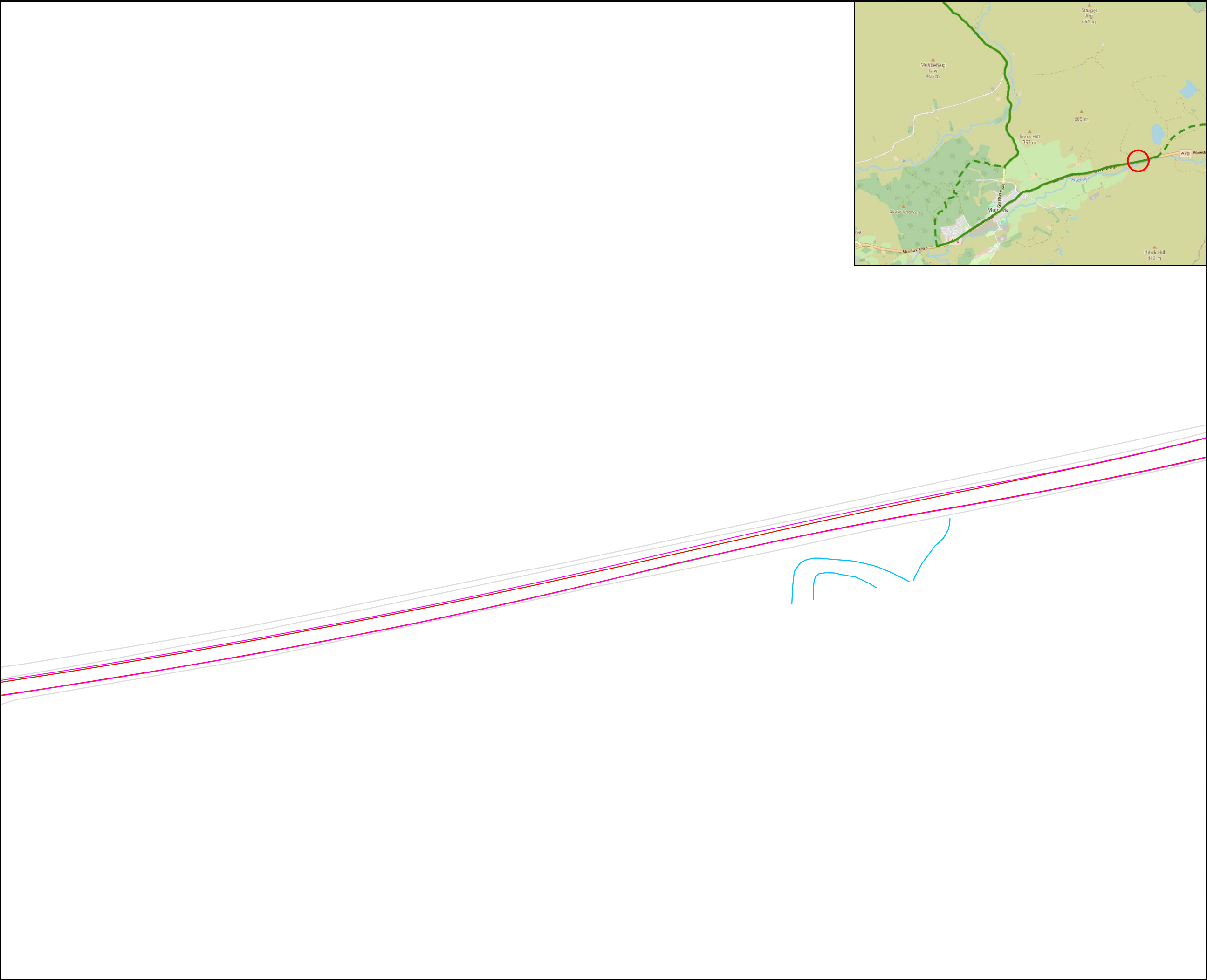
Client
3R Energy

Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 01

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000

Drawing Status Information	Drawing Number GB01T24G63/2001/I/S/SPA001	Rev. -
-------------------------------	--	-----------



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

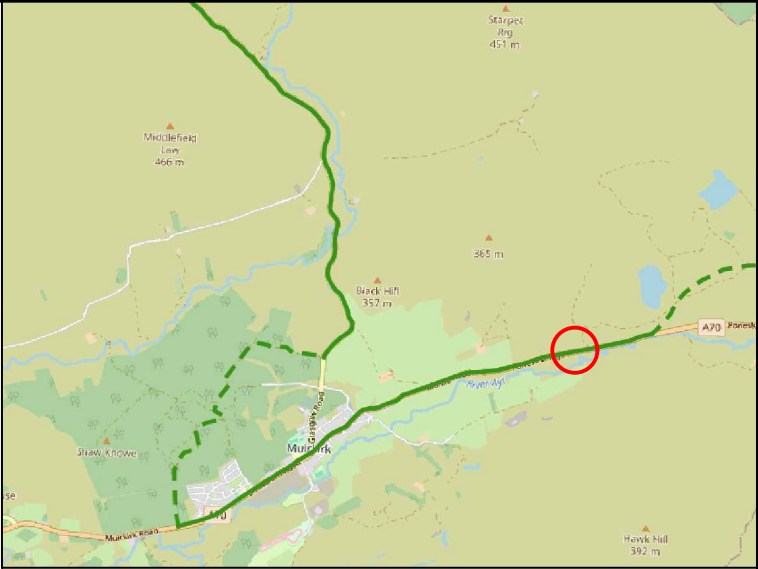
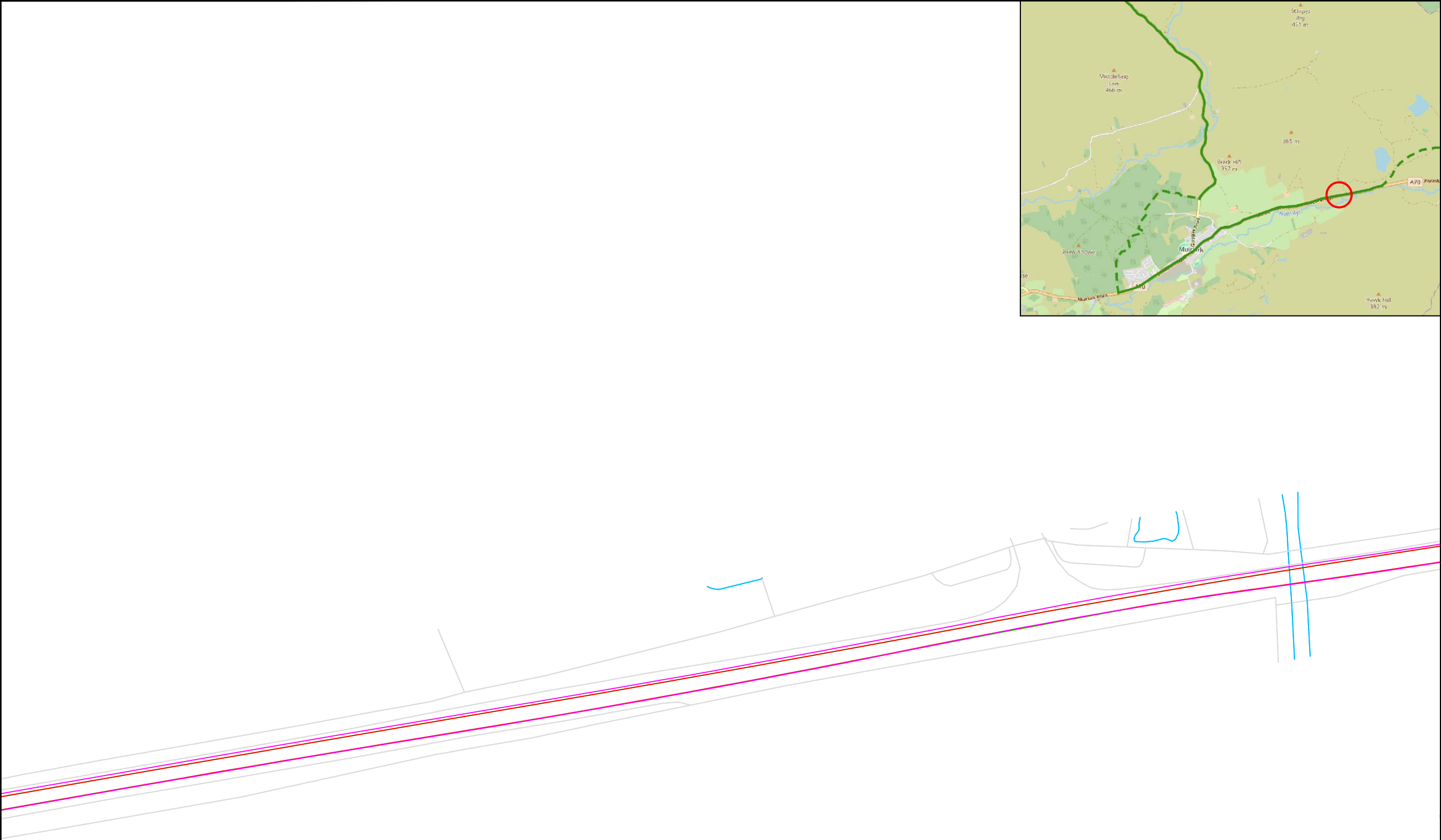
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 03

Drawn TF	Checked ADV	Approved ADV
Original dtd. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA003

Rev.
-



- Notes:
1. Do not scale from drawing.
 2. All dimensions in metres unless otherwise stated.
 3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.



- Key:
- Public Highway
 - Off-road Private Access Tracks
 - Blade Transporter Vehicle
 - Blade Outline
 - Over-run envelope of tractor and trailer wheels
 - Over-sail envelope being swept by tractor and trailer bed
 - Over-sail envelope being swept by blade path
 - Over-run area of tractor and trailer wheels out with carriageway
 - Over-sail area of tractor and trailer bed out with carriageway
 - Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

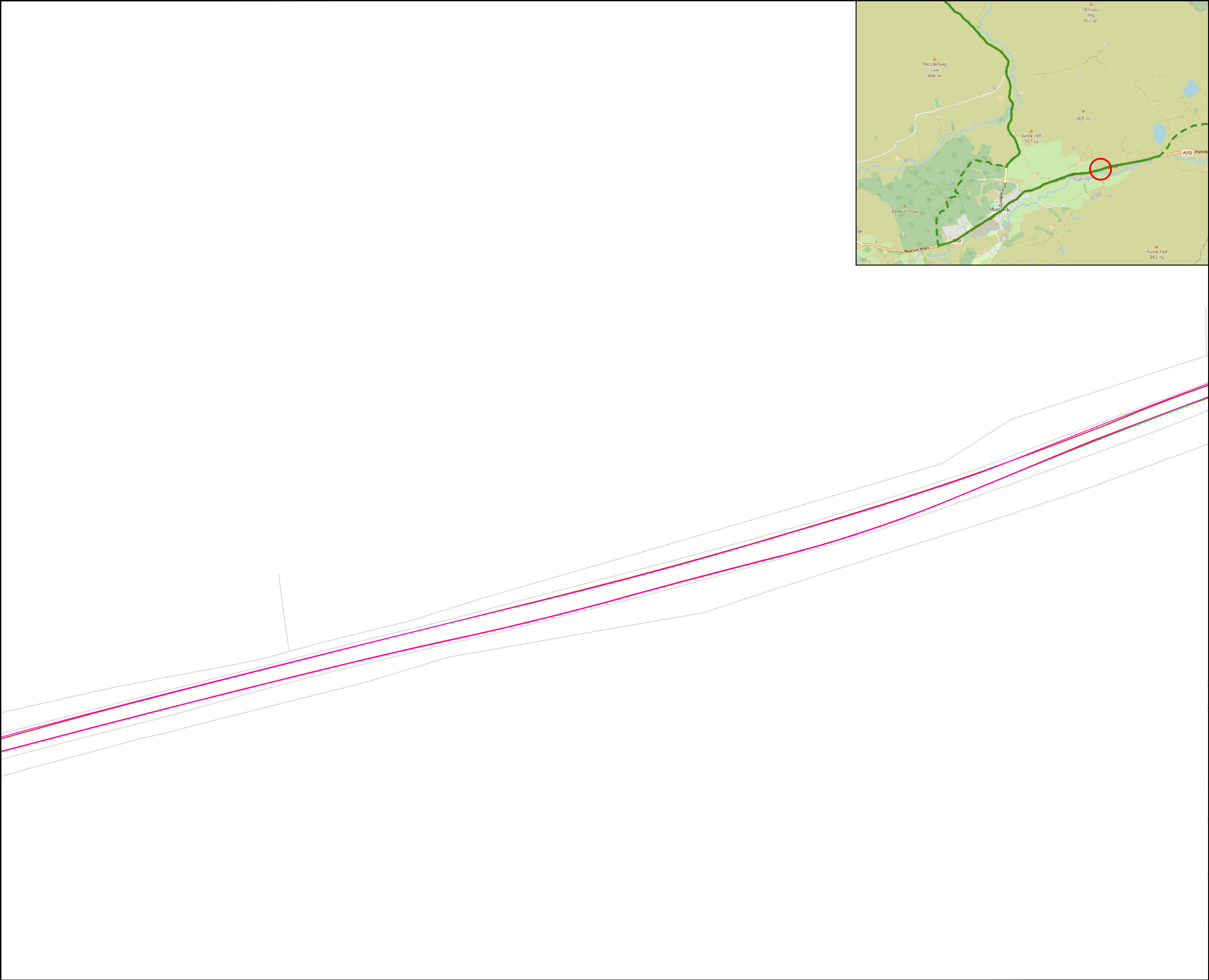
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 04

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information	Drawing Number	GB01T24G63/2001/I/S/SPA004	Rev.	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

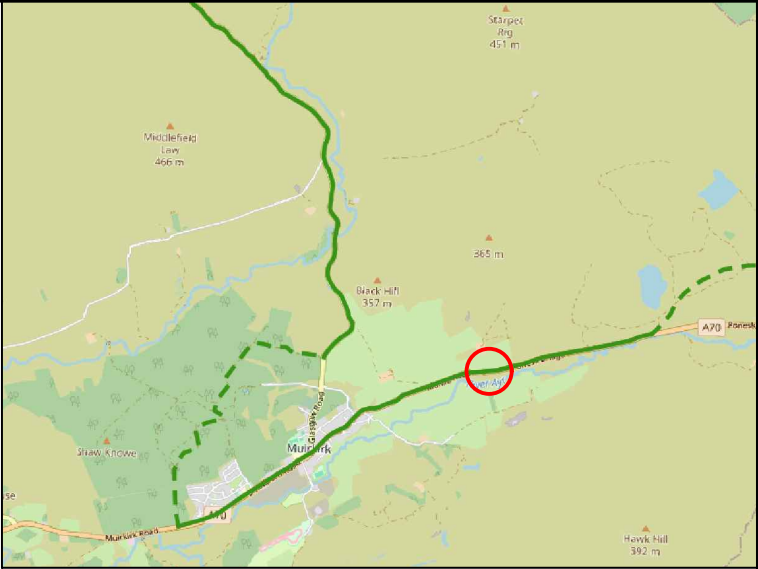
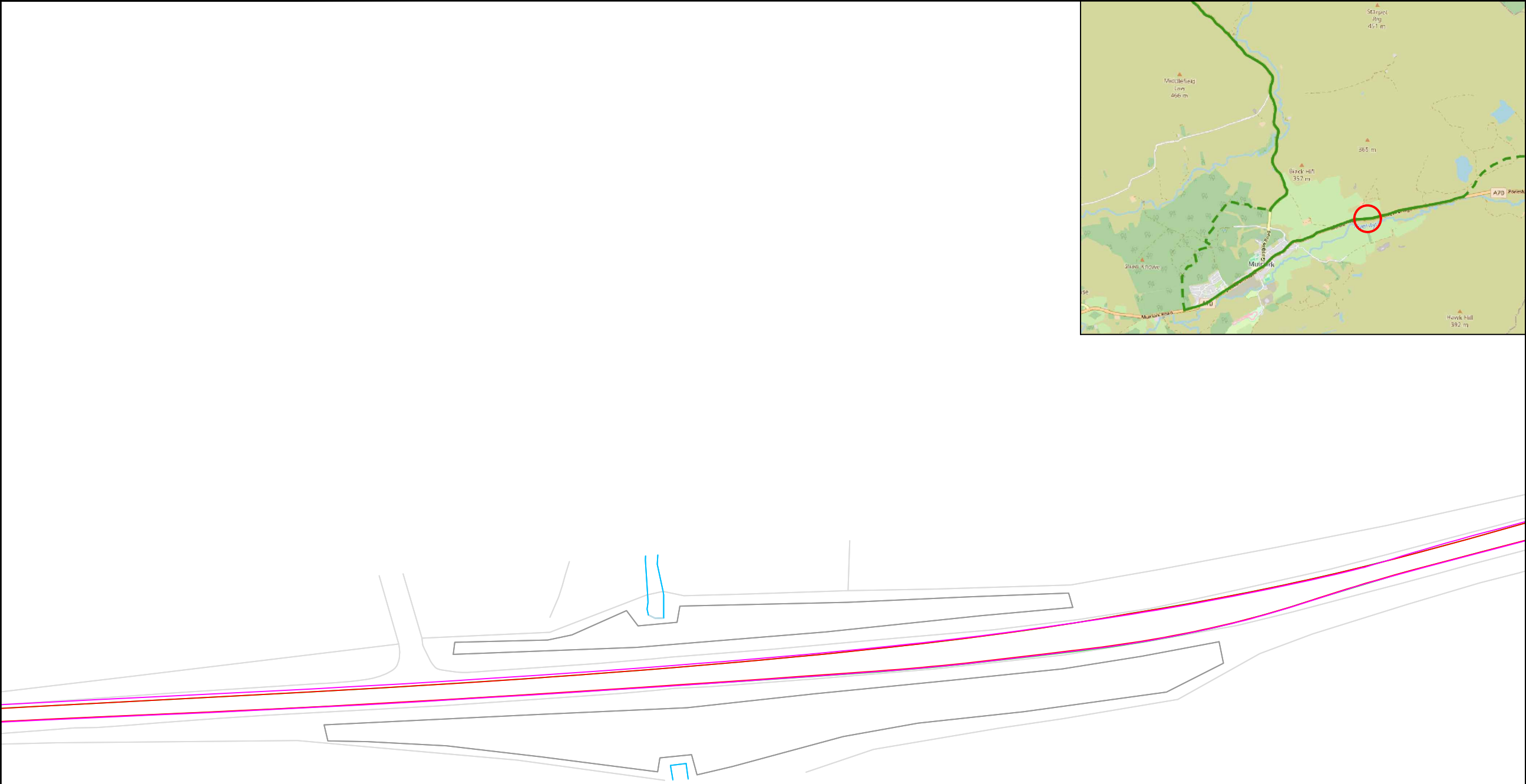
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 06

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA006	Rev.

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



- Notes:**
1. Do not scale from drawing.
 2. All dimensions in metres unless otherwise stated.
 3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.



- Key:**
- Public Highway
 - Off-road Private Access Tracks
 - Blade Transporter Vehicle
 - Blade Outline
 - Over-run envelope of tractor and trailer wheels
 - Over-sail envelope being swept by tractor and trailer bed
 - Over-sail envelope being swept by blade path
 - Over-run area of tractor and trailer wheels out with carriageway
 - Over-sail area of tractor and trailer bed out with carriageway
 - Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

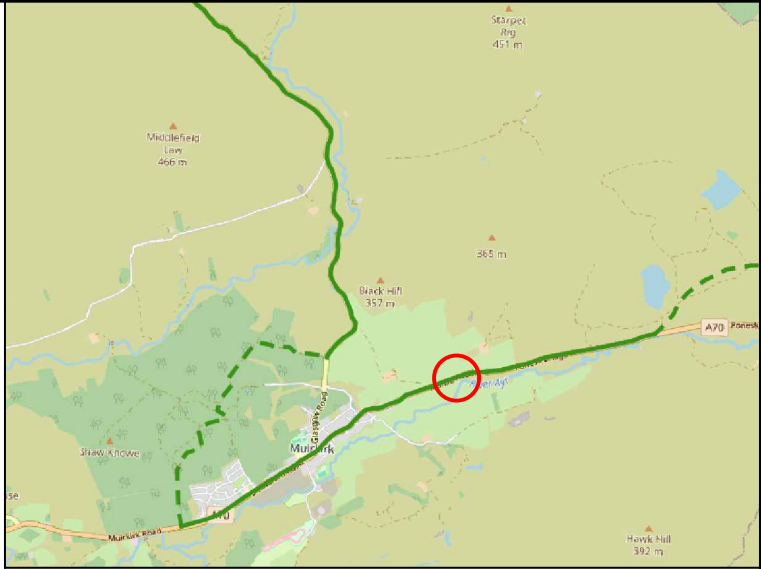
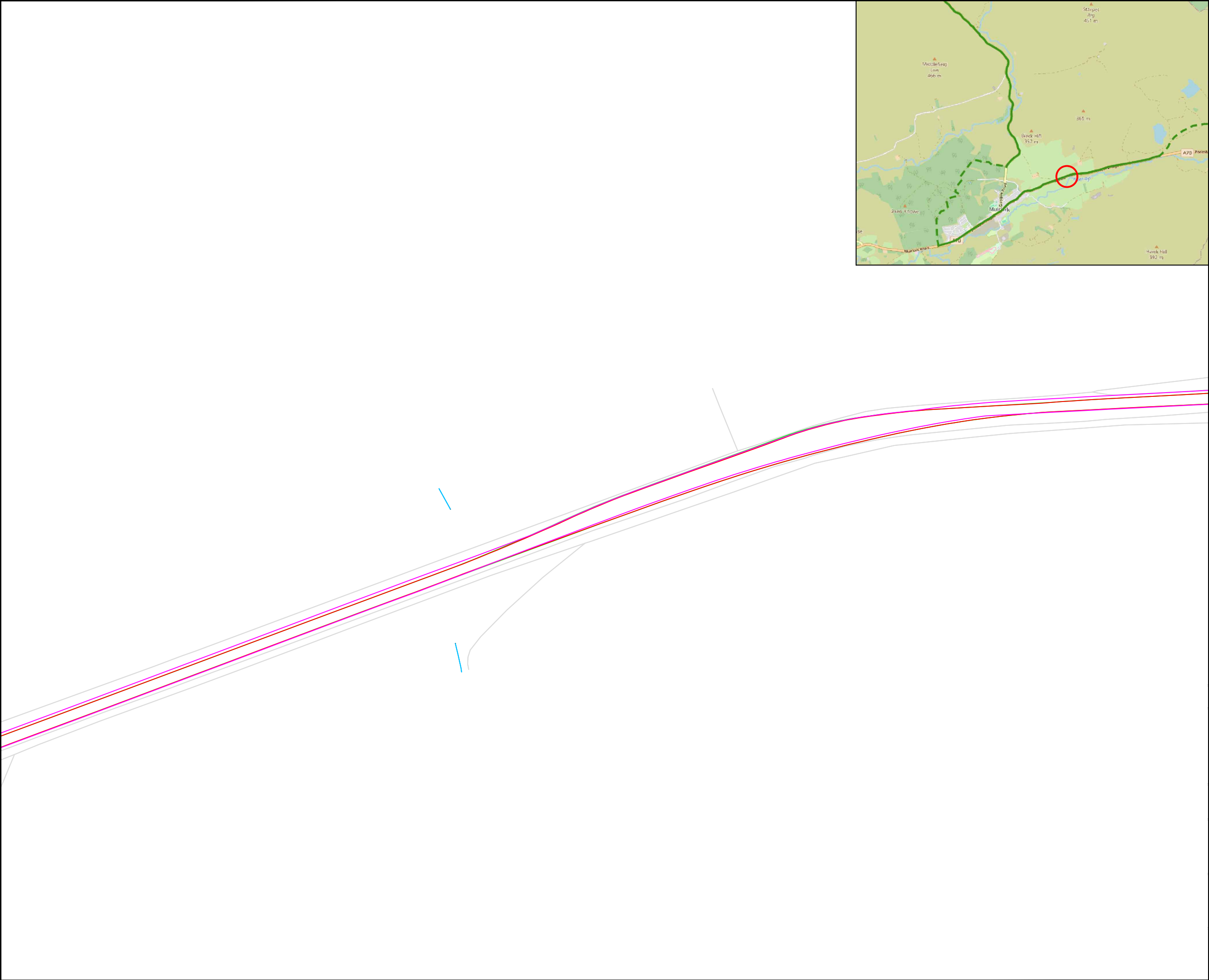
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 07

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information	Drawing Number	GB01T24G63/2001/I/S/SPA007	Rev.	-



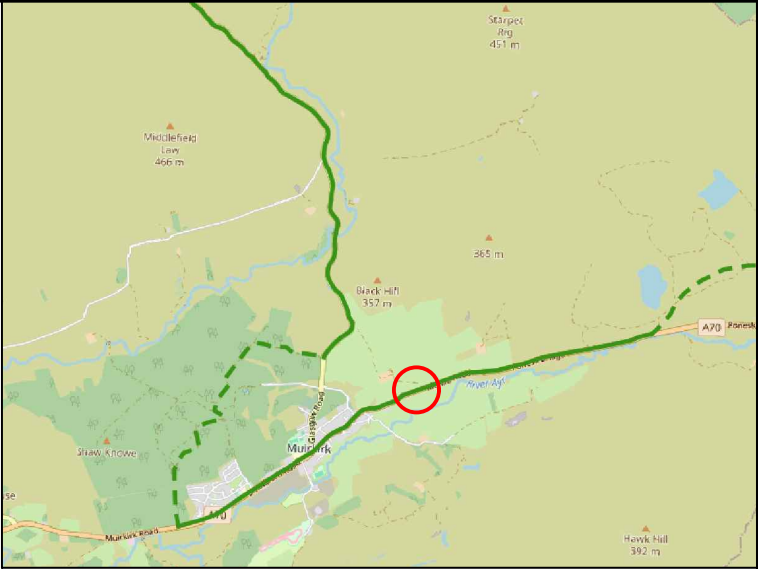
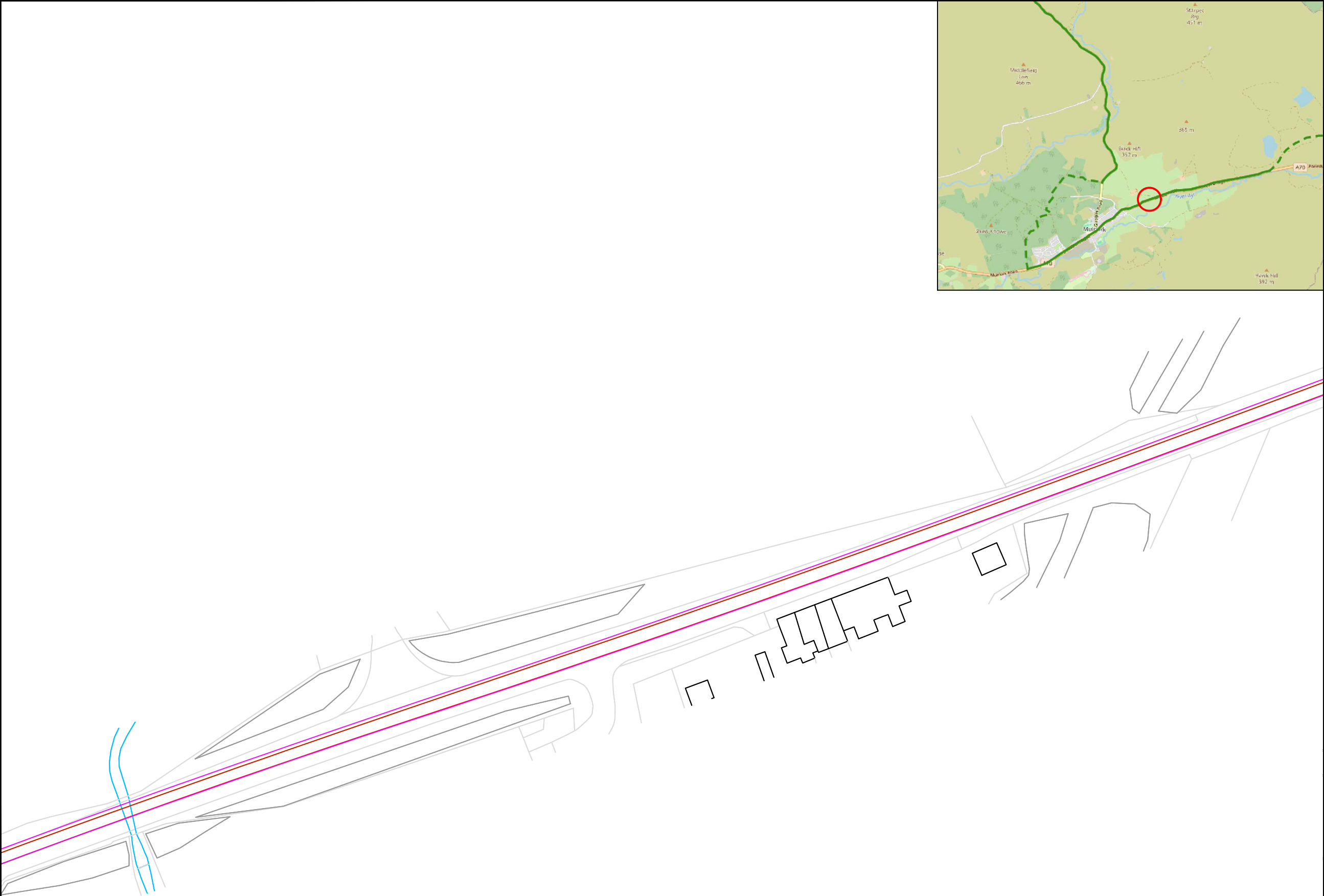
Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved
© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.					
<div><div>SYSTRA www.systra.com/uk</div><div>19 Charlotte Square, Edinburgh, EH2 4DF</div></div> <div><div>T 0131 460 1847</div><div>E scotland@systra.co.uk</div></div>					
Client 3R Energy					
Project Hagshaw Energy Cluster Western Expansion					
Title Swept Path Assessment 80.5m Blade "Super Wing" Blade Transport Pinch Point 08					
Drawn TF		Checked ADV		Approved ADV	
Original dwg. size A3		Date 17/03/2025		Scale 1:1000	
Drawing Status Information		Drawing Number GB01T24G63/2001/I/S/SPA008		Rev. -	



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

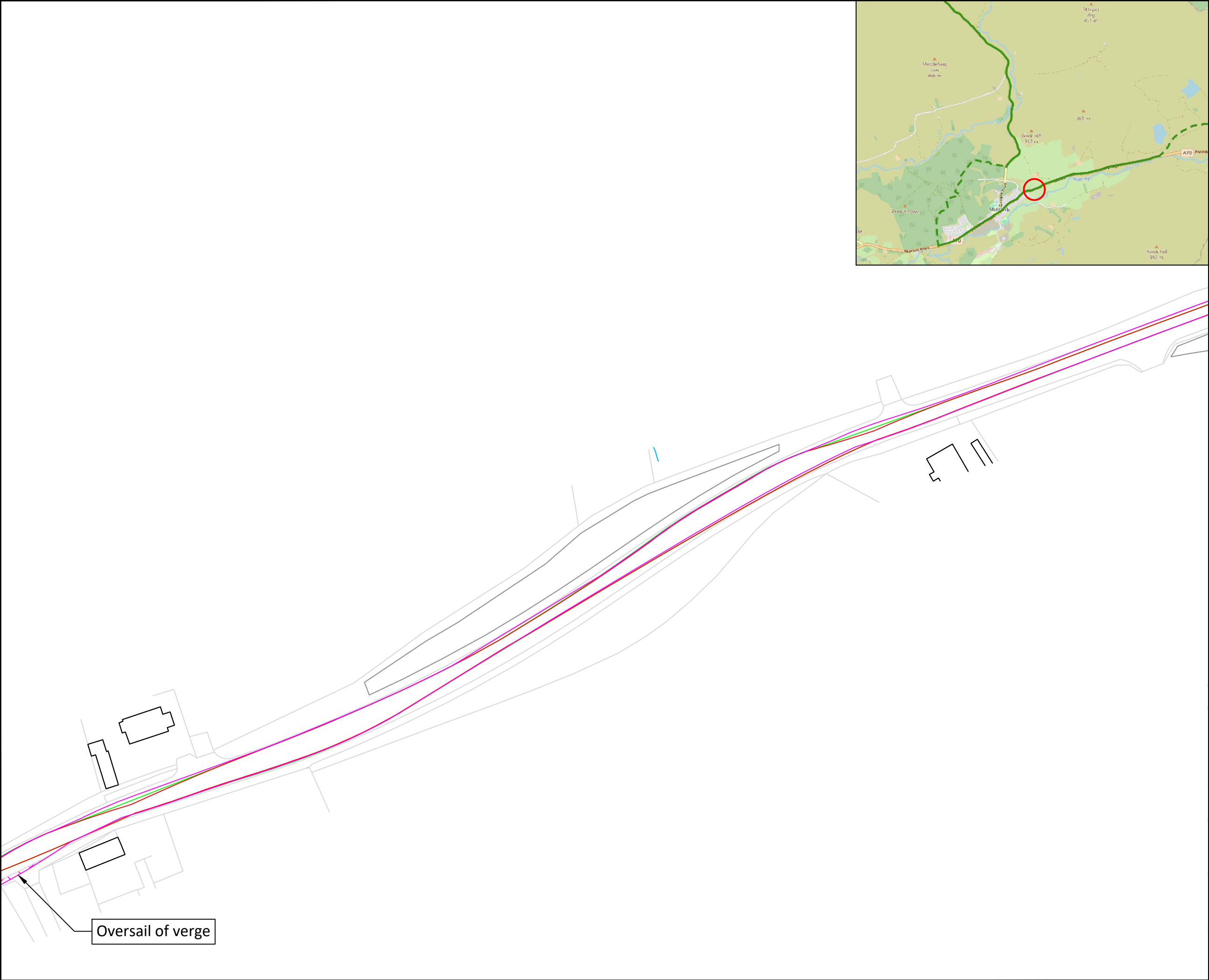
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 09

Drawn	Checked	Approved
TF	ADV	ADV
Original dtp. size	Date	Scale
A3	17/03/2025	1:1000
Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA009	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

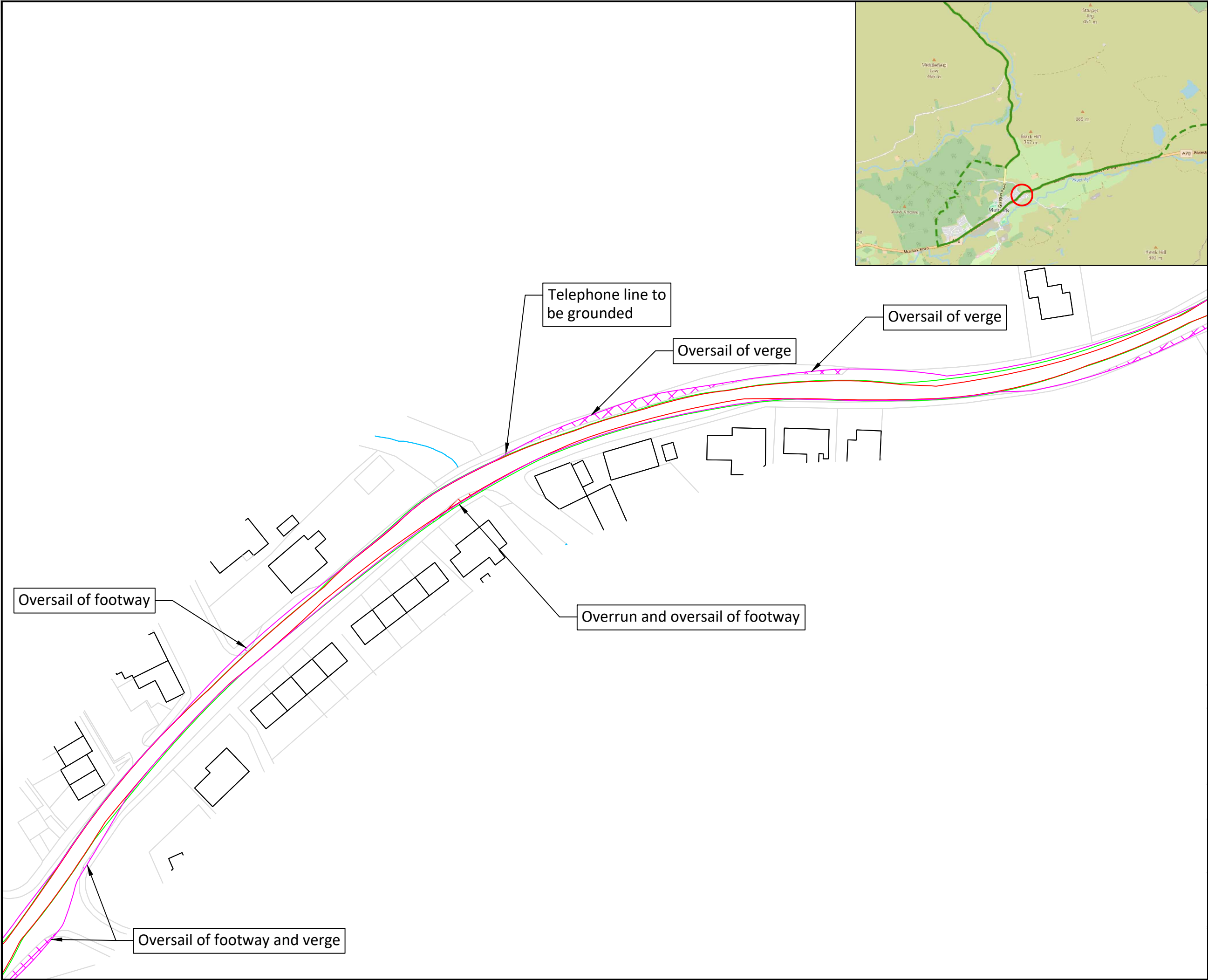
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 10

Drawn TF	Checked ADV	Approved ADV
Original dtp. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA010

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

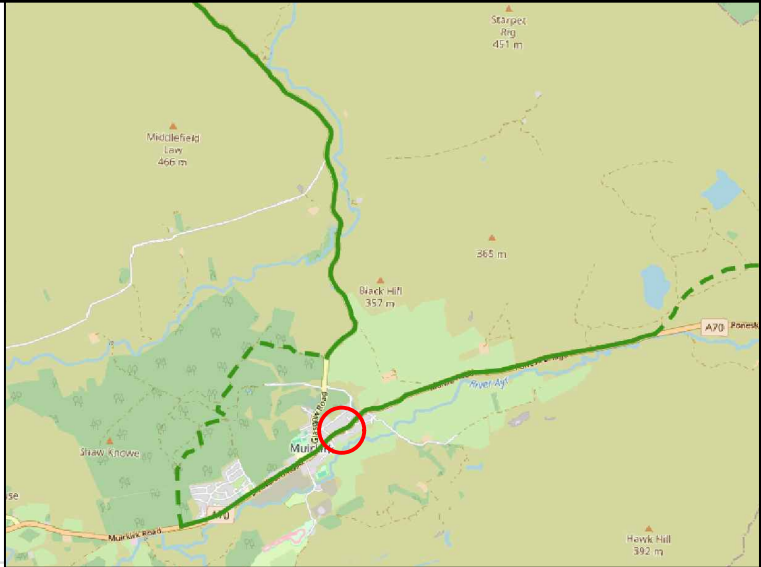
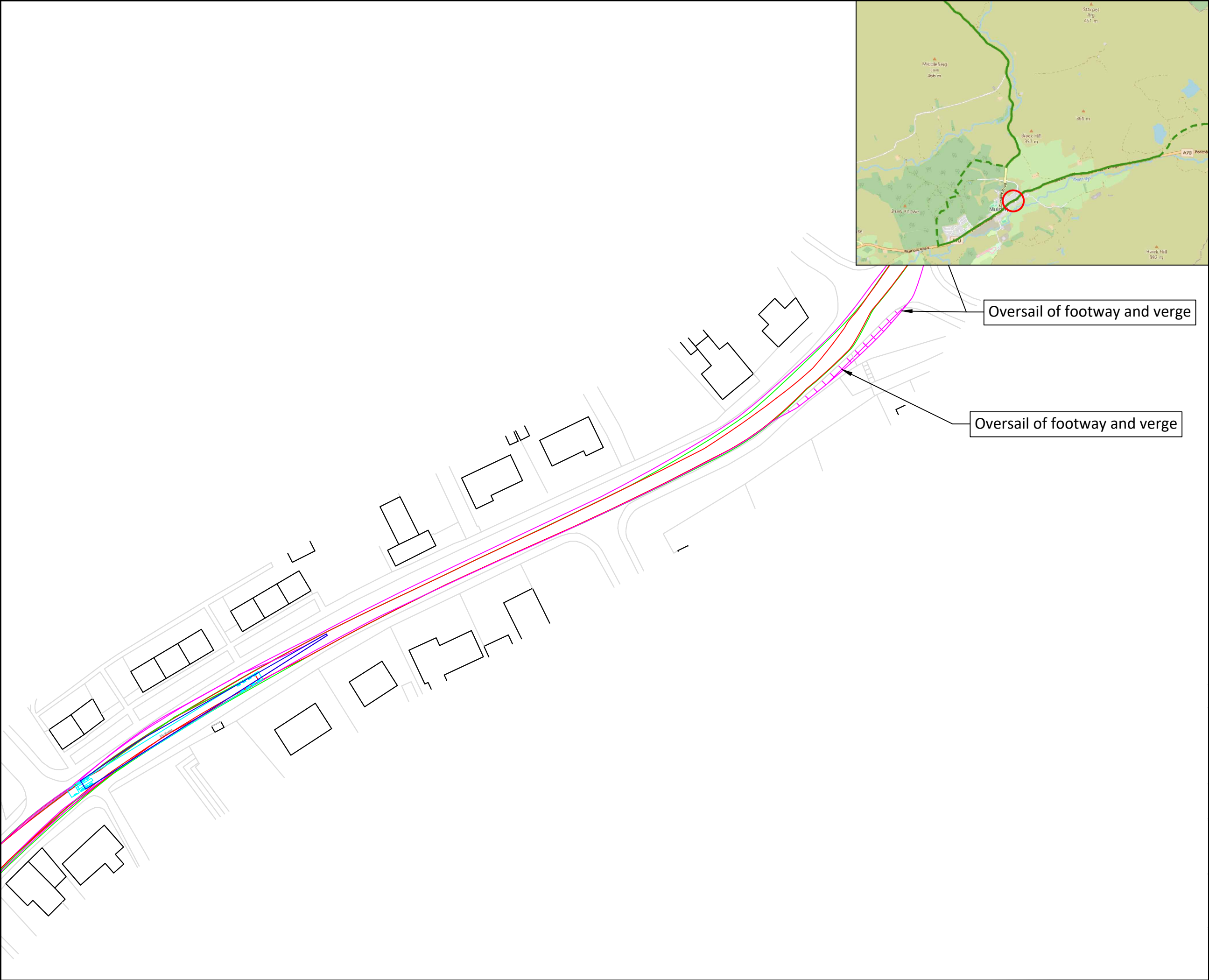
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 11

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA011	Rev.

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

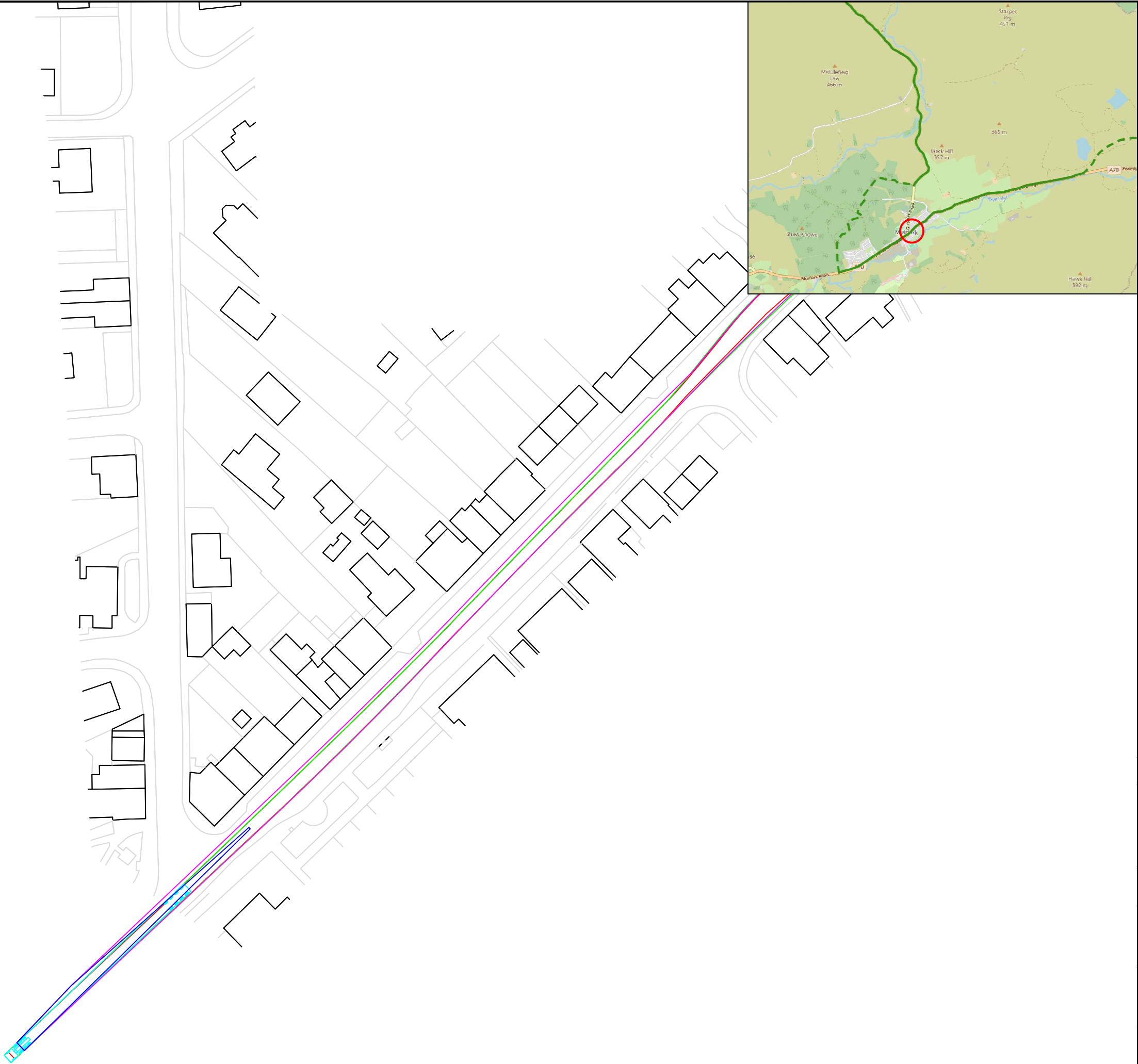
Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 12

Drawn	Checked	Approved
TF	ADV	ADV
Original dwg. size	Date	Scale
A3	17/03/2025	1:1000

Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA012	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

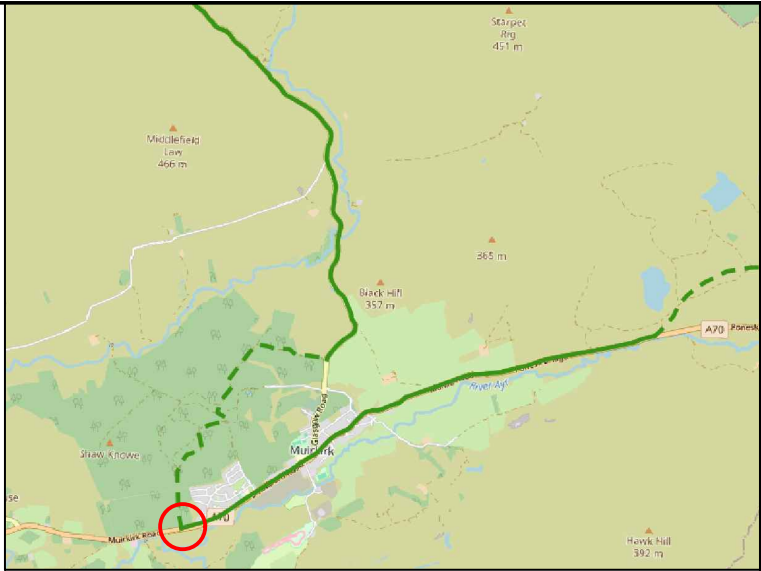
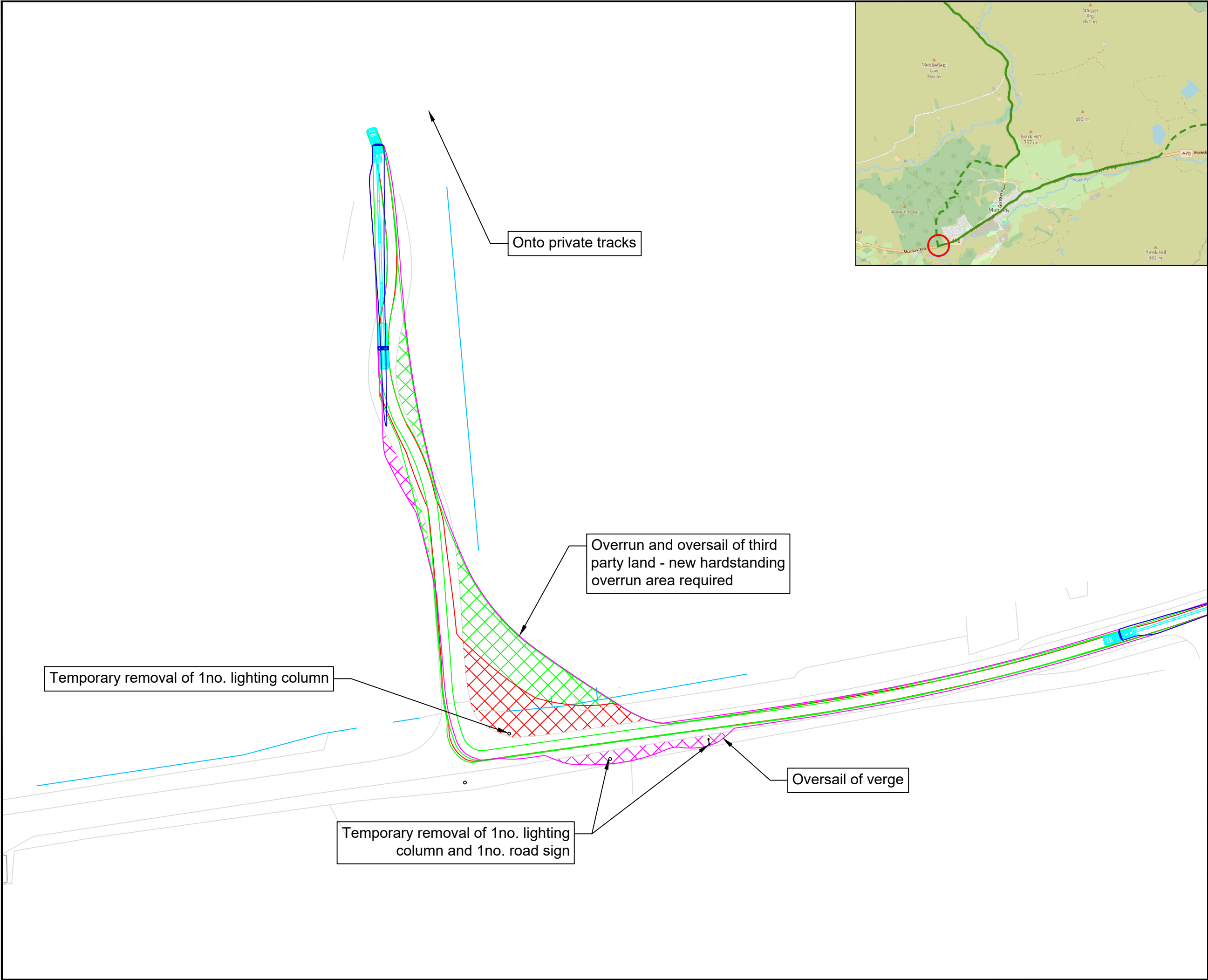
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 13

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA013

Rev.
-



Notes:

1. Do not scale from drawing.
2. All dimensions in metres unless otherwise stated.
3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

18 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

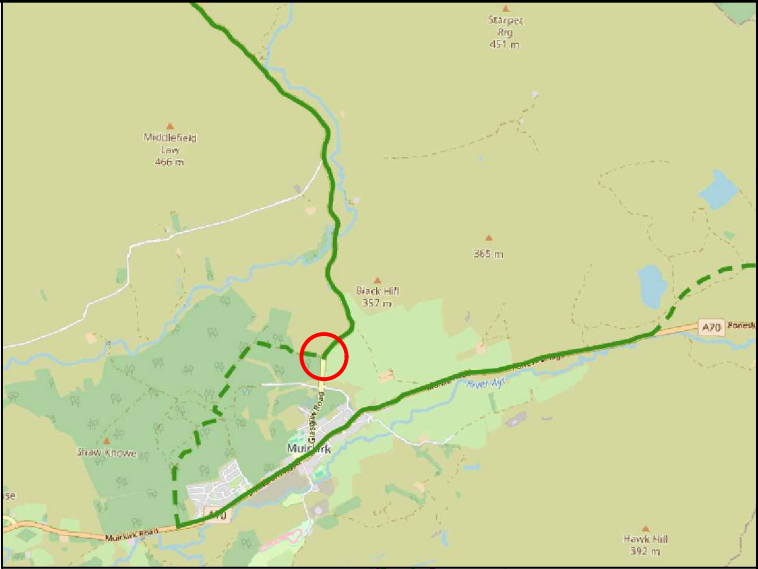
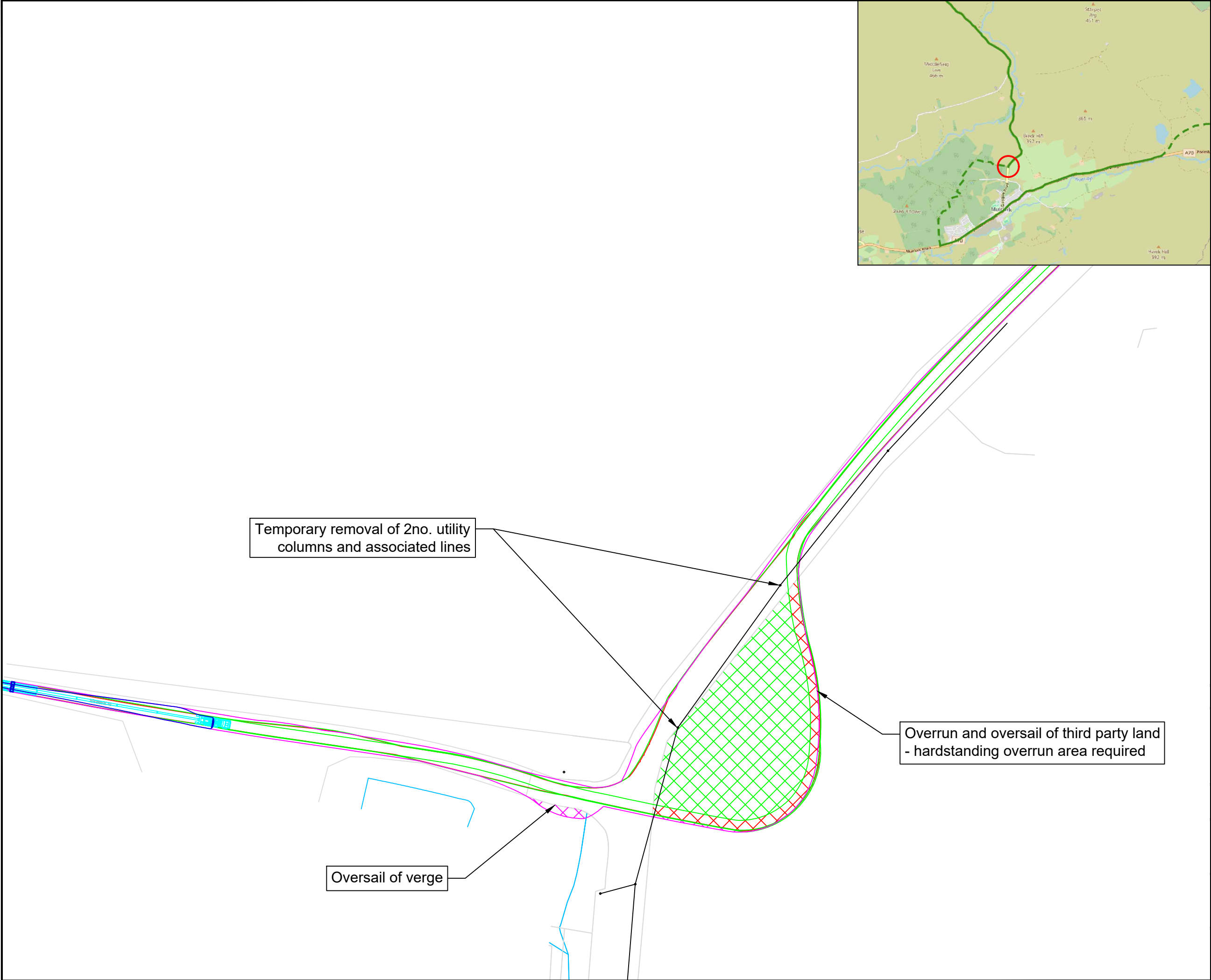
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 14

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA014

Rev.
-



Notes:

1. Do not scale from drawing.
2. All dimensions in metres unless otherwise stated.
3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

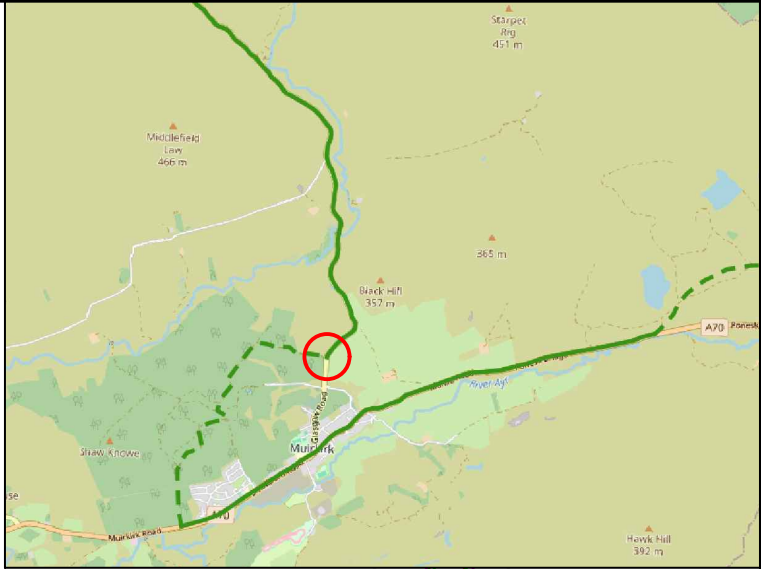
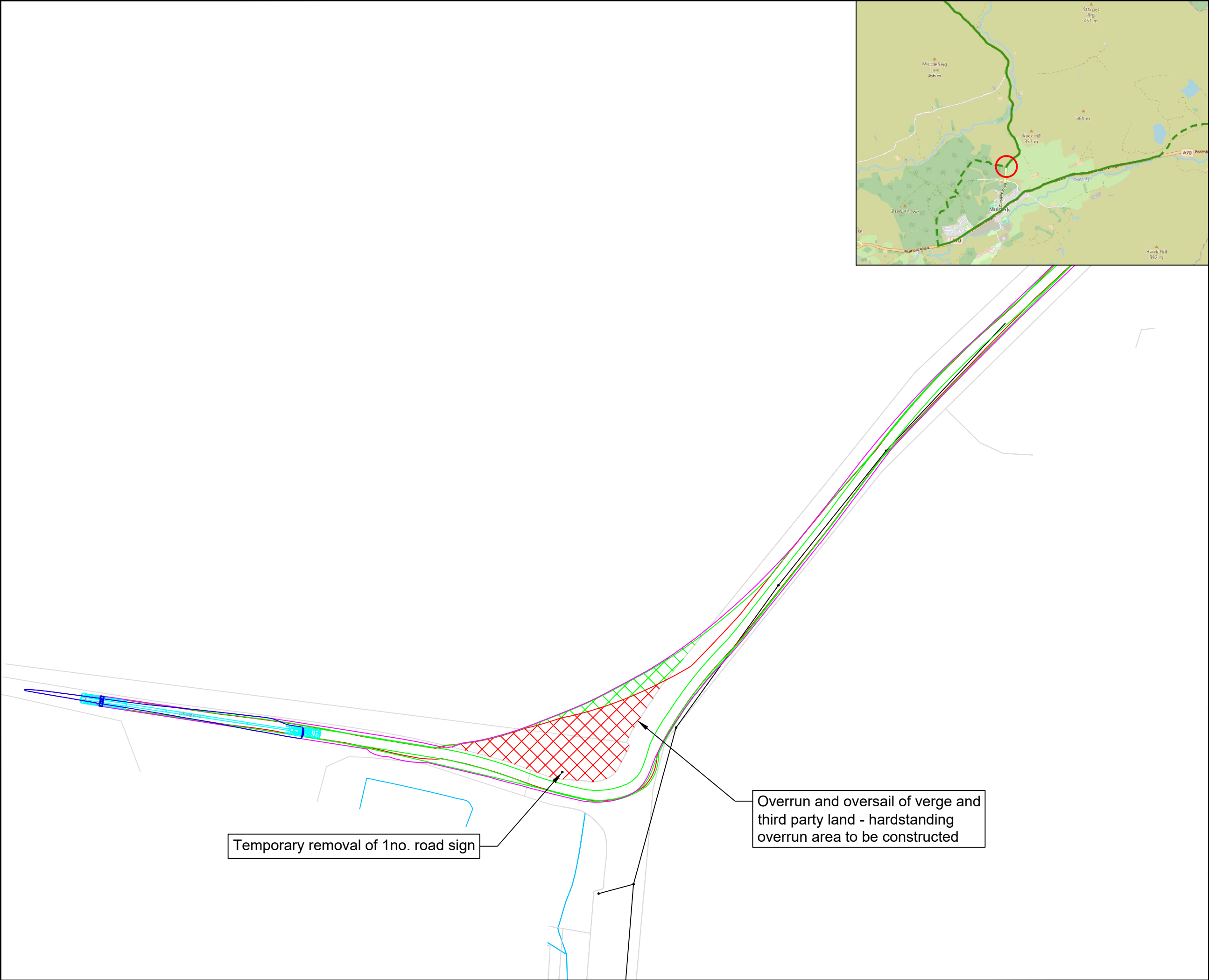
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 15 - Outside Turn

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA015A

Rev.
-



Notes:

1. Do not scale from drawing.
2. All dimensions in metres unless otherwise stated.
3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

18 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

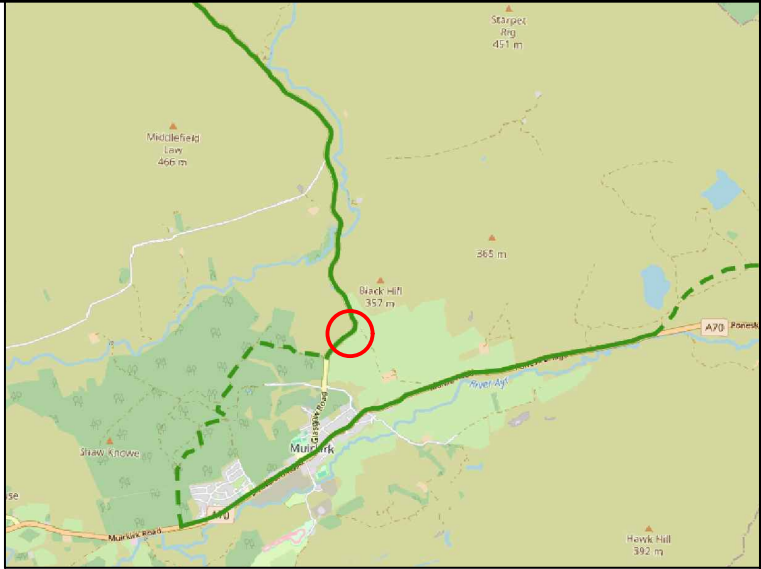
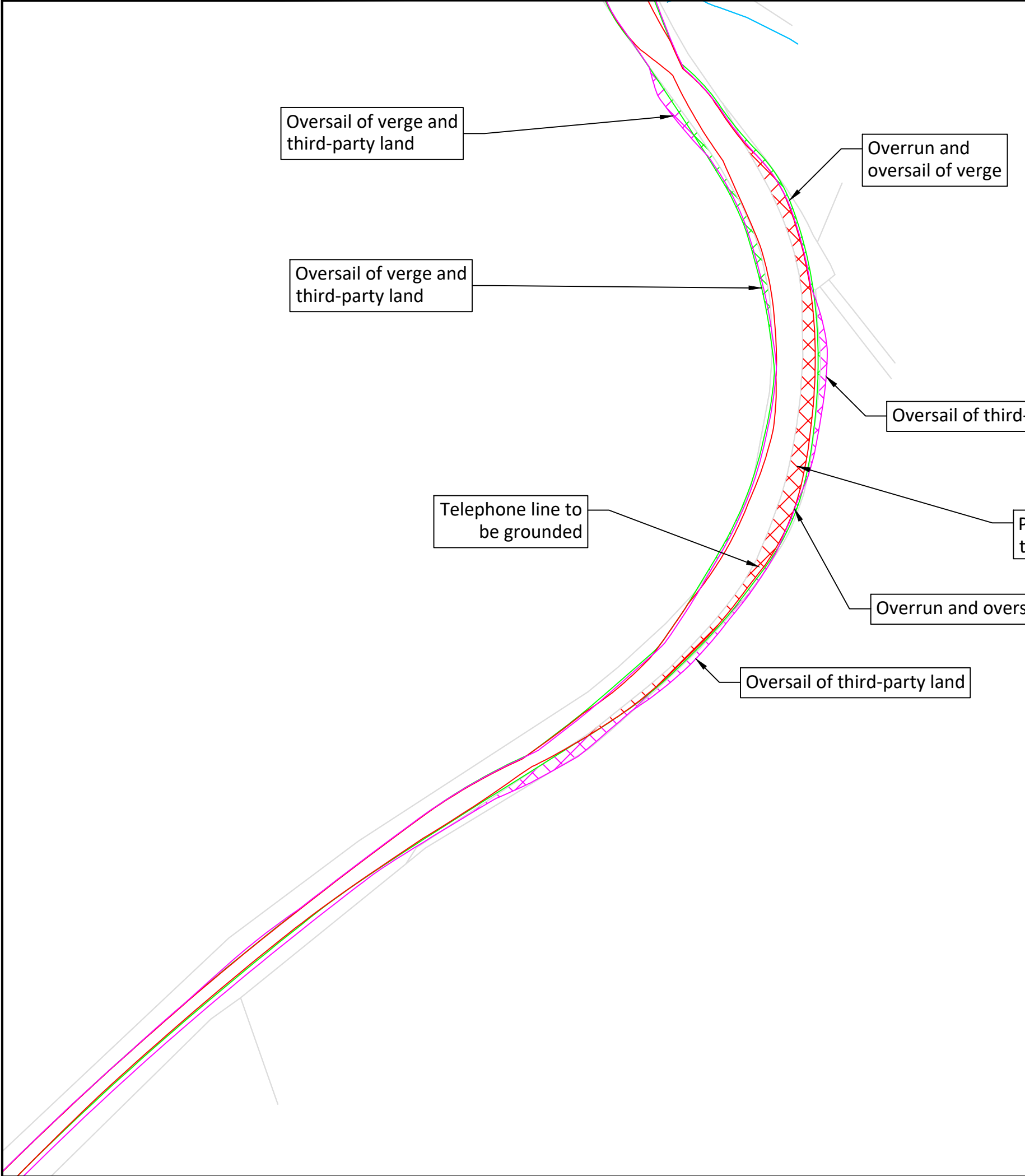
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 15 - Inside Turn

Drawn	Checked	Approved
TF	ADV	ADV
Original dwg. size	Date	Scale
A3	17/03/2025	1:1000
Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA015B	-

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

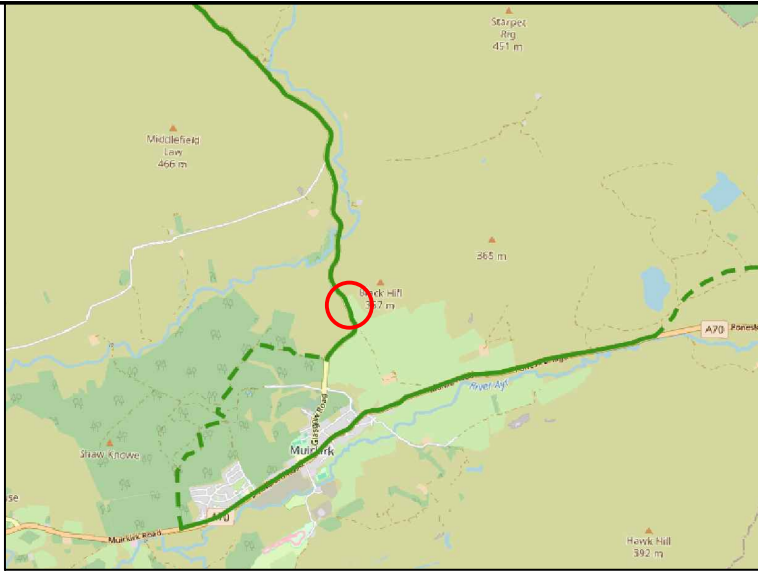
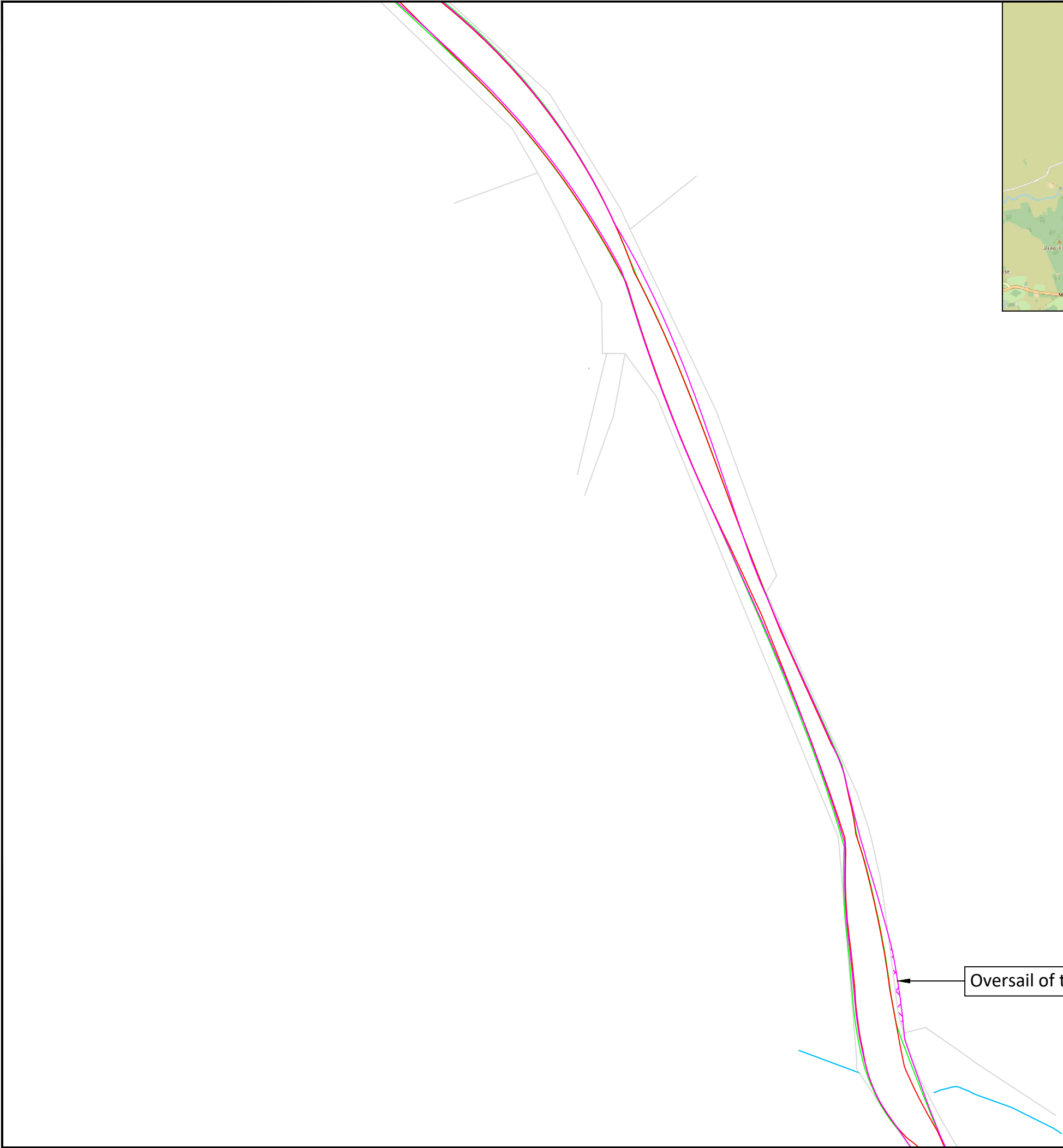
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 16

Drawn TF	Checked ADV	Approved ADV
Original dtp. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA016

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

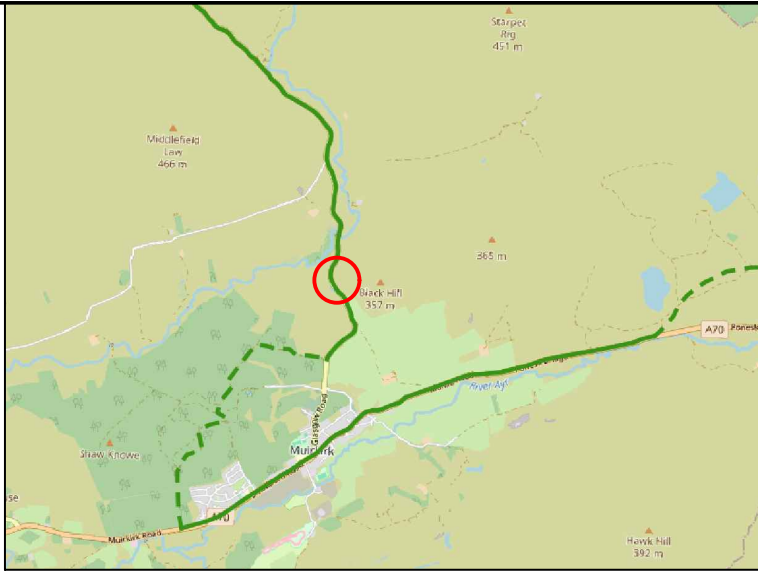
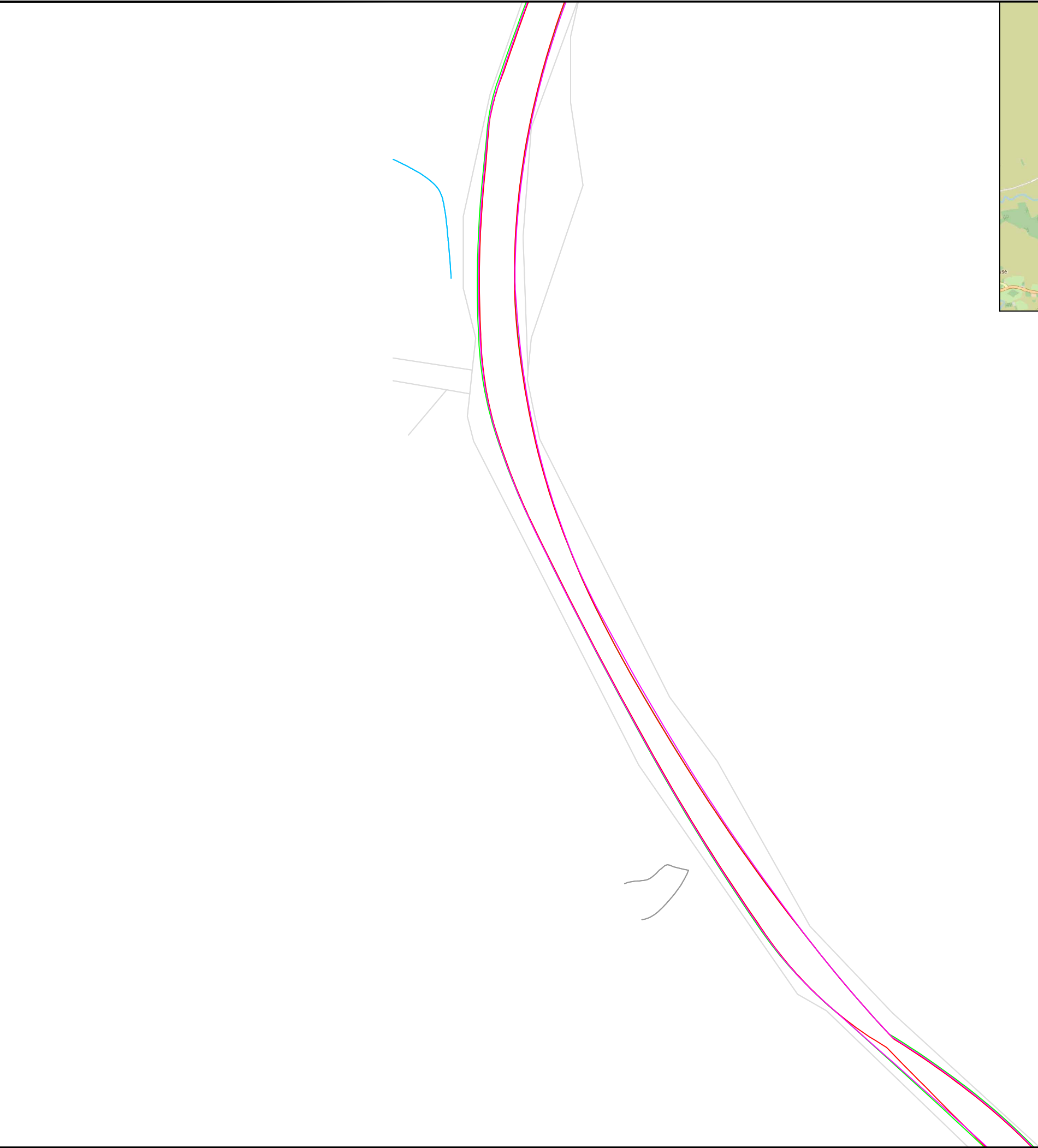
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 17

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA017	Rev.



Notes:


- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

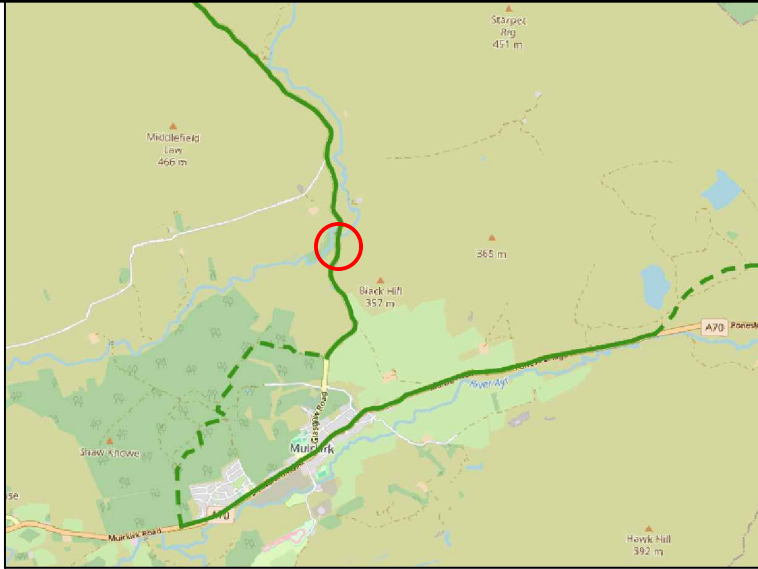
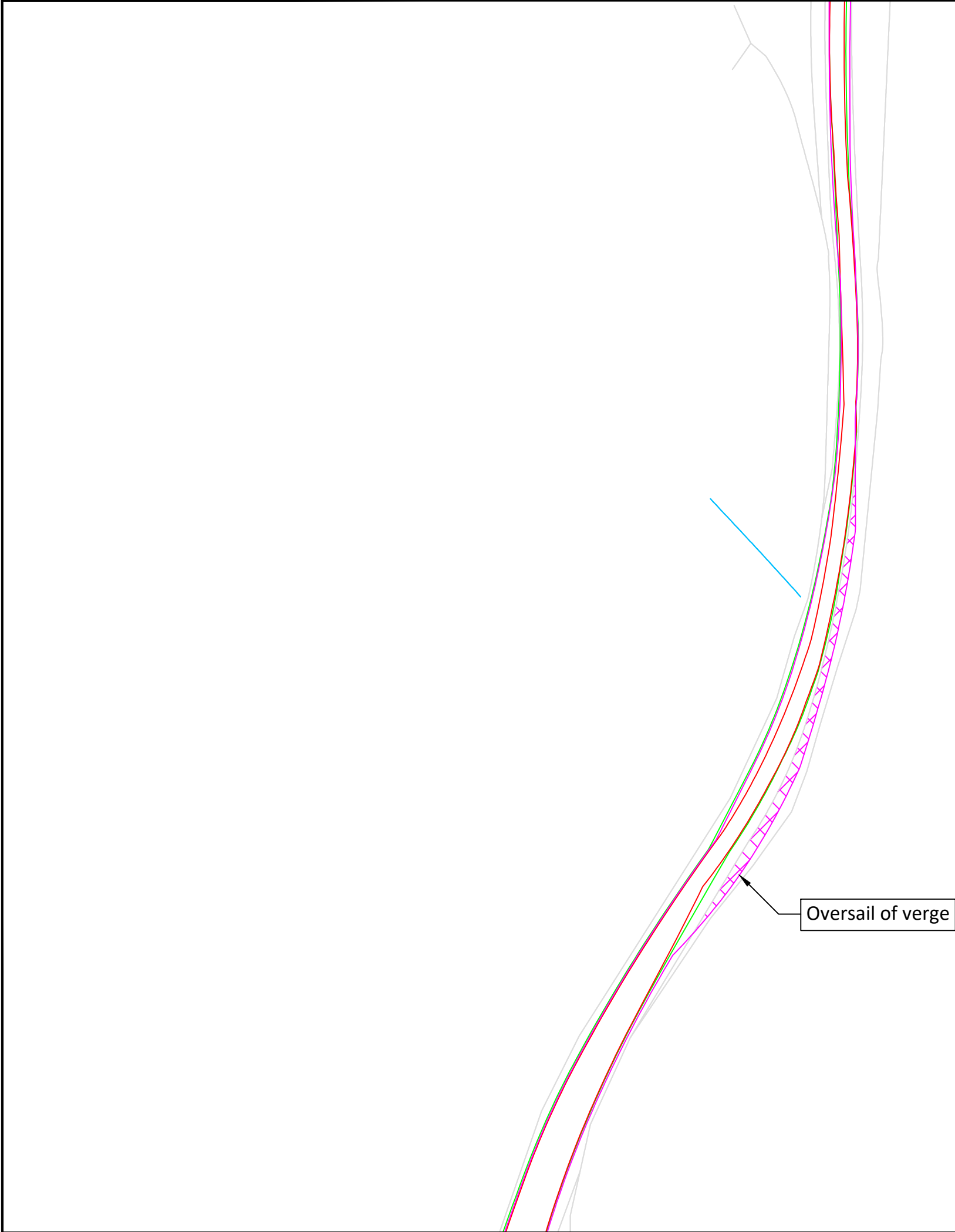
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 18

Drawn	Checked	Approved
TF	ADV	ADV
Original dwg. size	Date	Scale
A3	17/03/2025	1:1000
Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA018	-

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

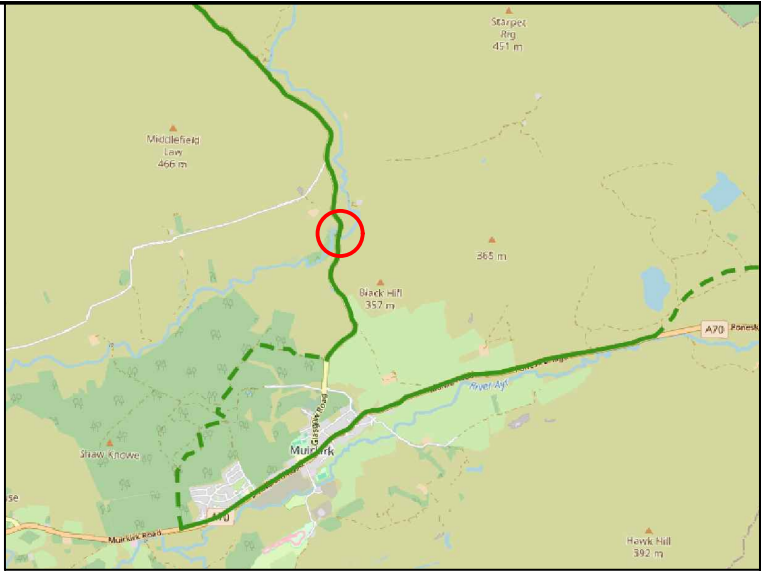
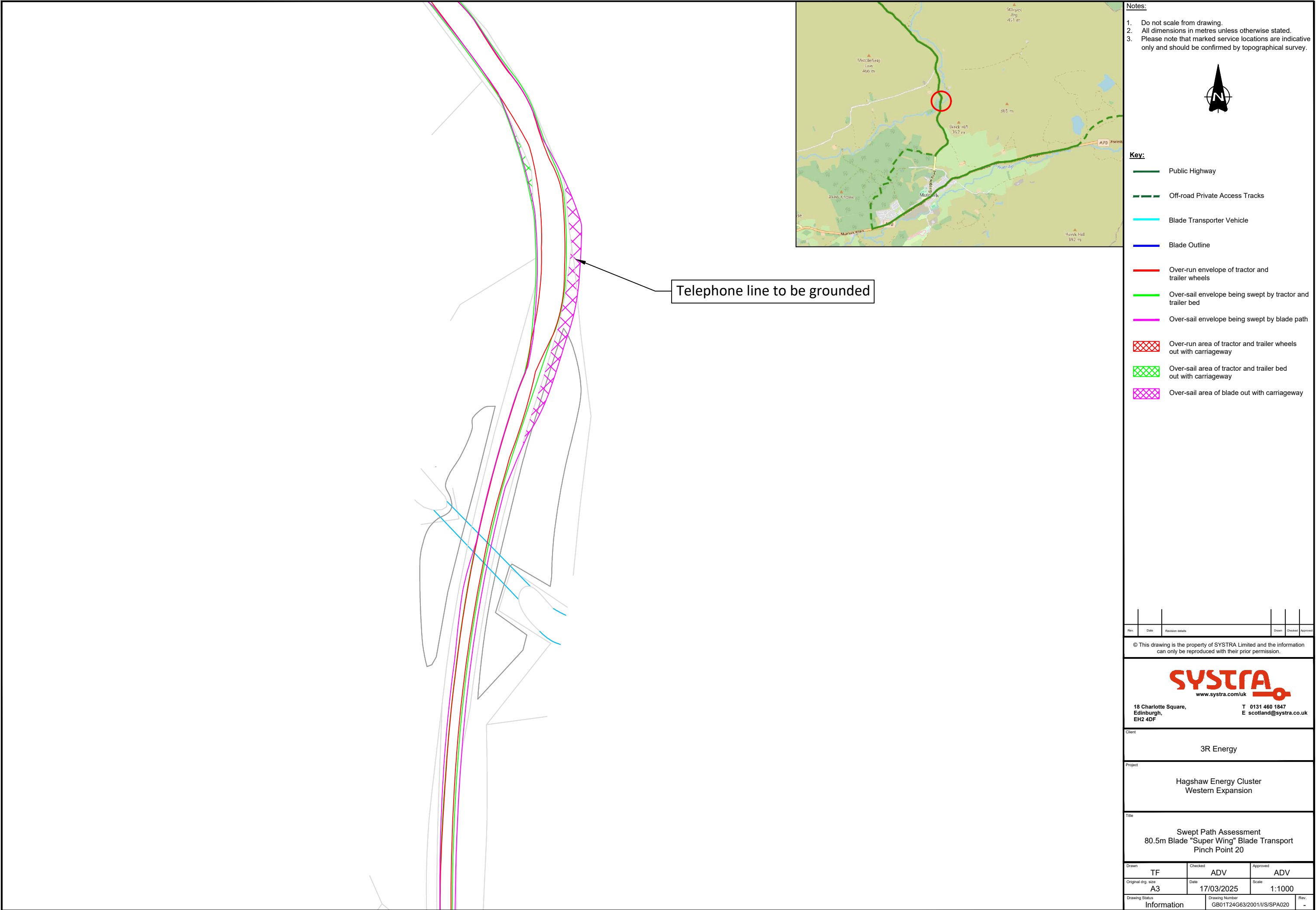
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 19

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA019	Rev.
					-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

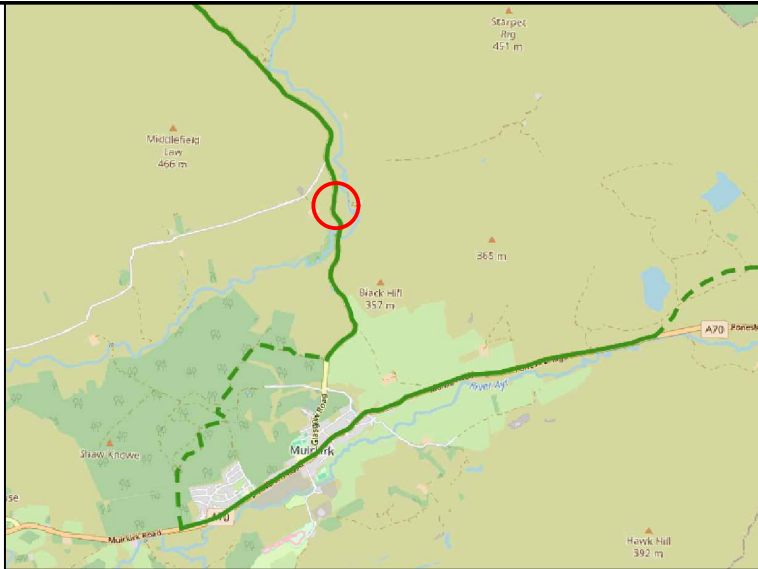
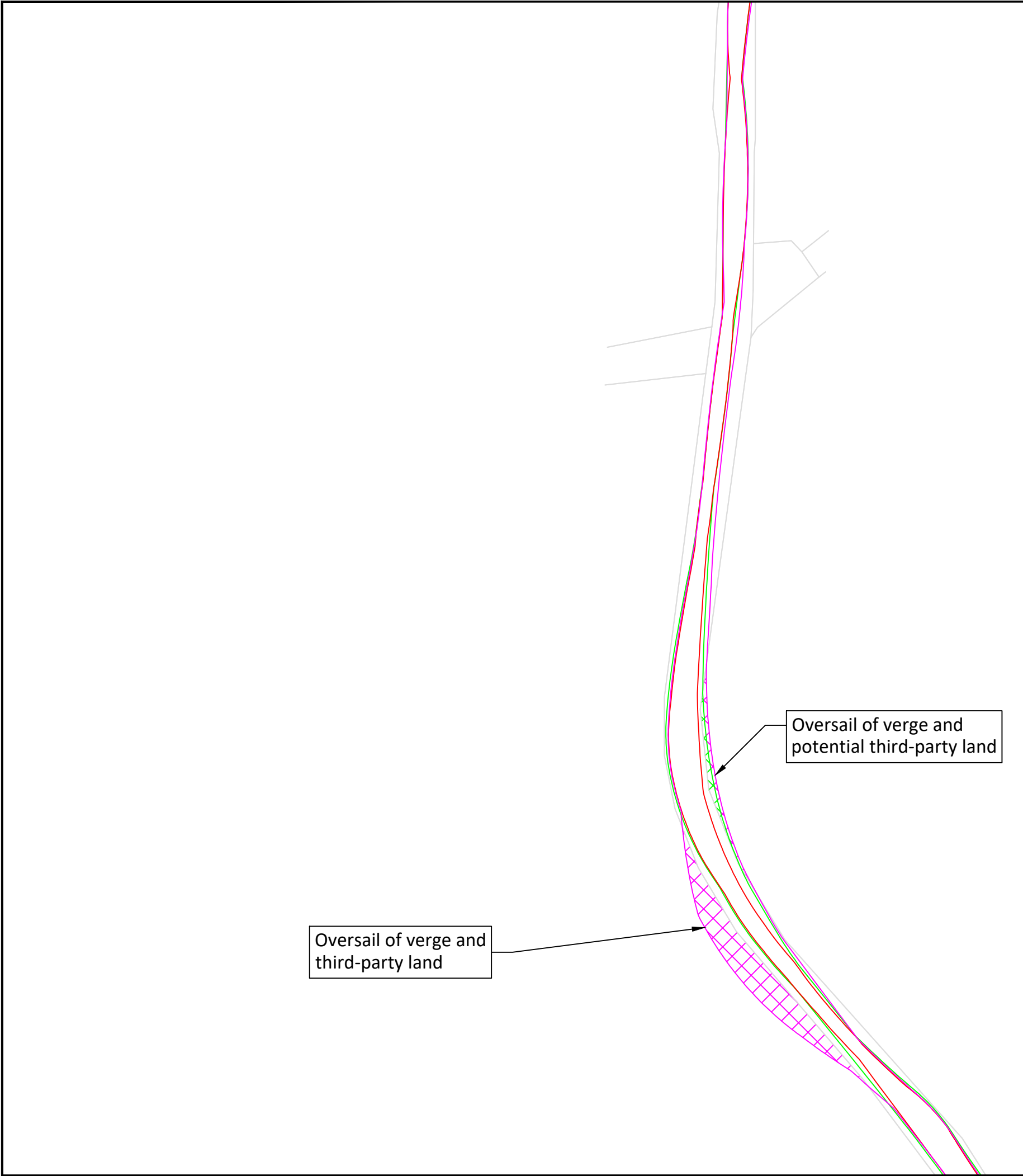
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 20

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA020	Rev.
					-

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 21

Drawn	Checked	Approved
TF	ADV	ADV
Original dwg. size	Date	Scale
A3	17/03/2025	1:1000

Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA021	-

Contains OS data © Crown copyright [and database rights] [2025]

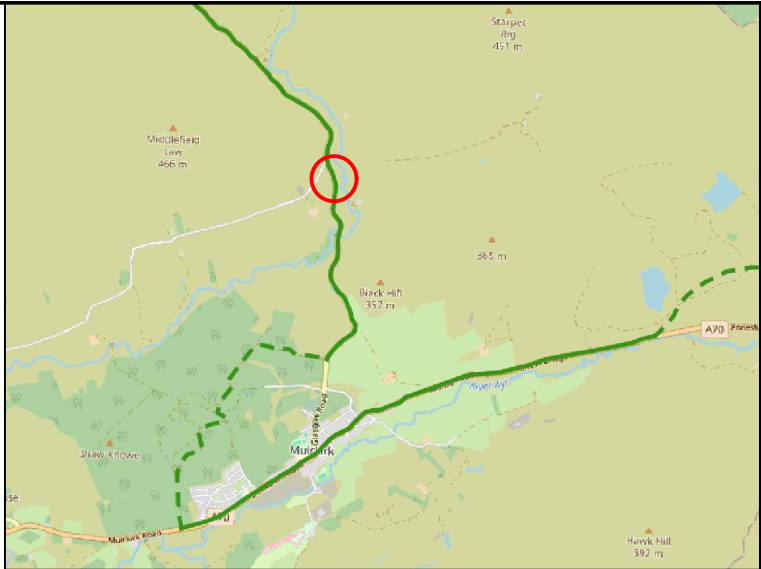
© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.

Oversail of verge and third-party land

Oversail of verge and third-party land

Oversail of verge and third-party land

Oversail of verge and third-party land



- Notes:
1. Do not scale from drawing.
 2. All dimensions in metres unless otherwise stated.
 3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.



- Key:
- Public Highway
 - Off-road Private Access Tracks
 - Blade Transporter Vehicle
 - Blade Outline
 - Over-run envelope of tractor and trailer wheels
 - Over-sail envelope being swept by tractor and trailer bed
 - Over-sail envelope being swept by blade path
 - Over-run area of tractor and trailer wheels out with carriageway
 - Over-sail area of tractor and trailer bed out with carriageway
 - Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

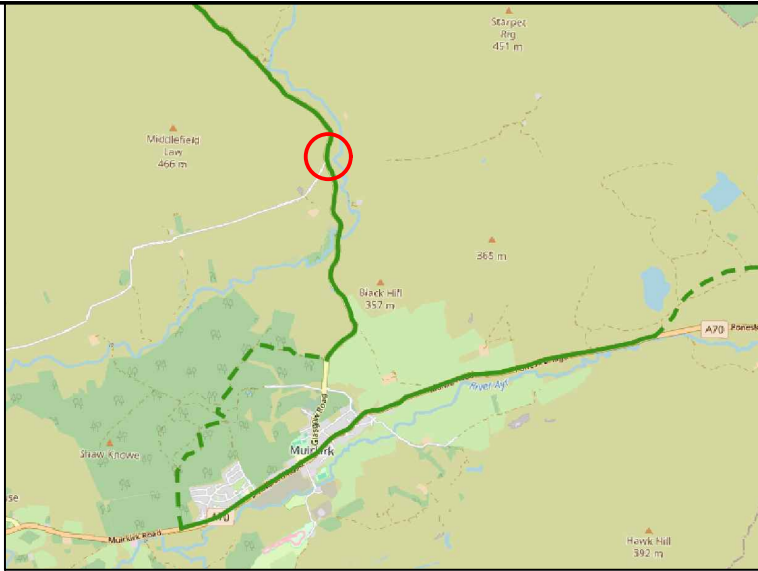
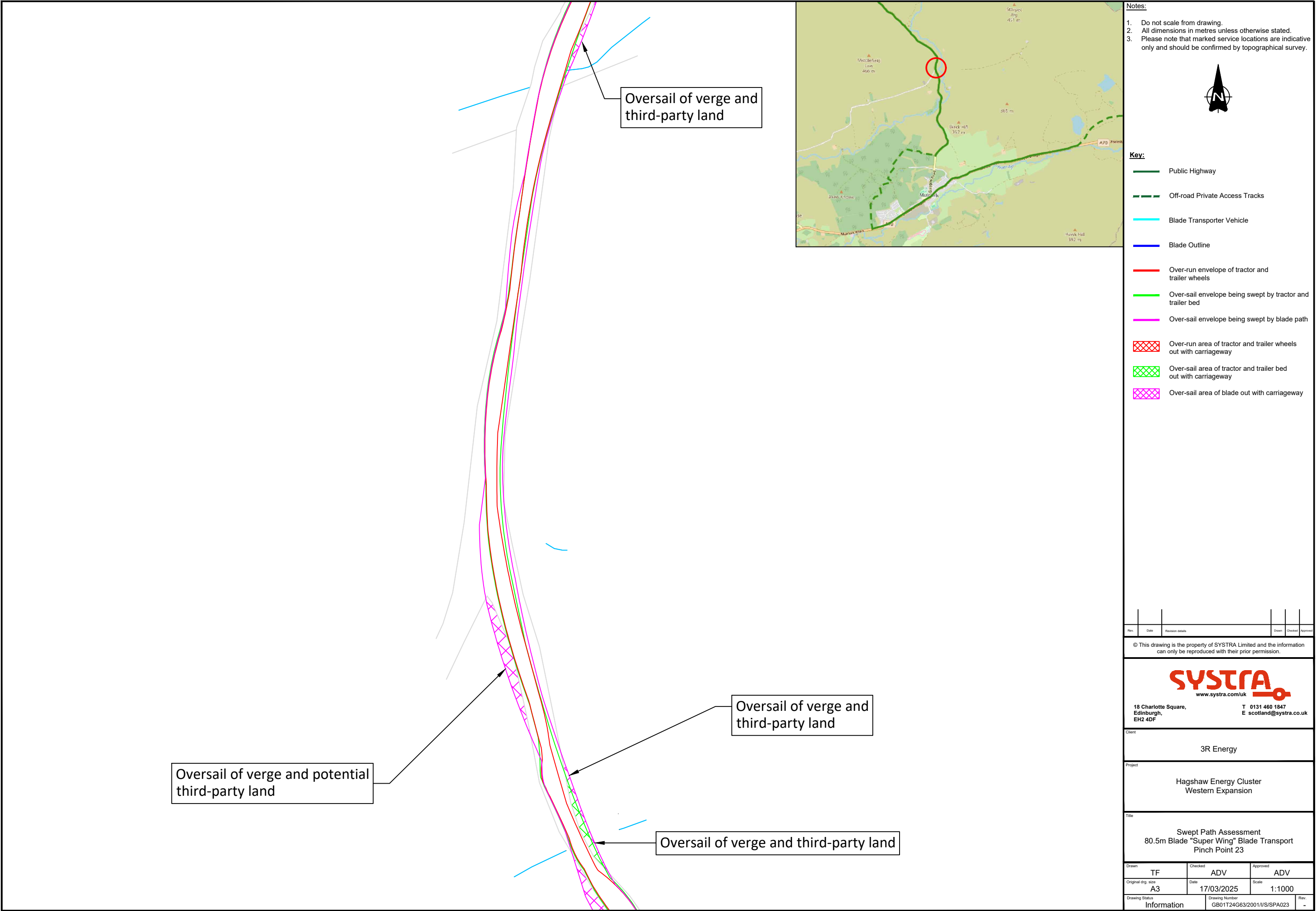
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 22

Drawn	TF	Checked	ADV	Approved	ADV
Original dtd. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA022	Rev.
					-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

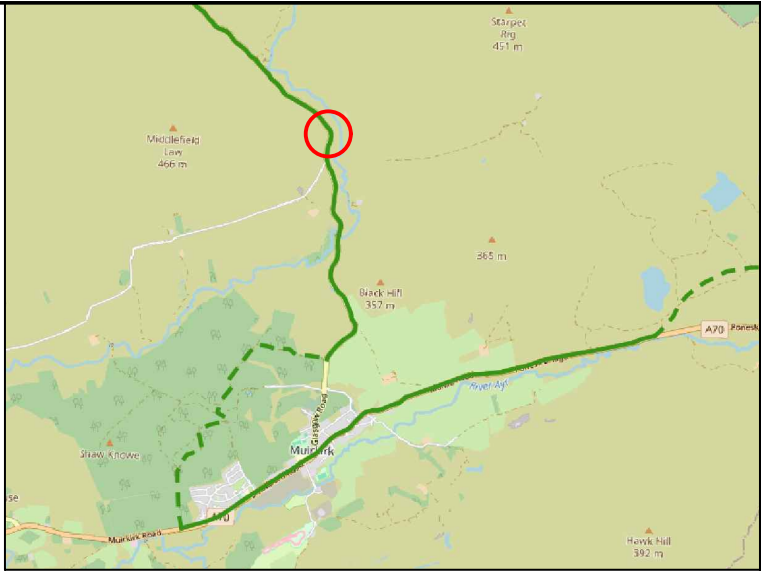
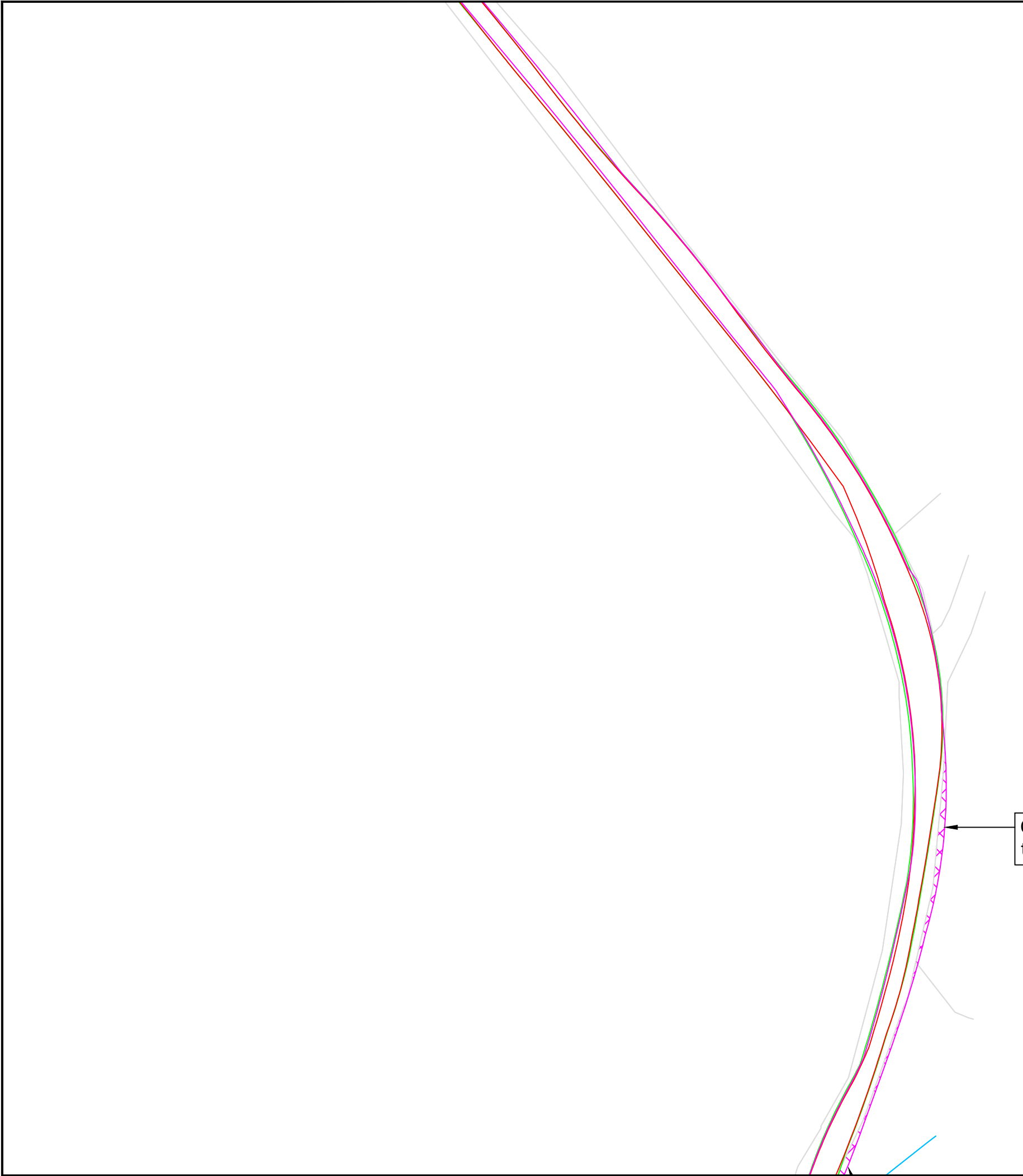
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 23

Drawn	Checked	Approved
TF	ADV	ADV
Original dtp. size	Date	Scale
A3	17/03/2025	1:1000
Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA023	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

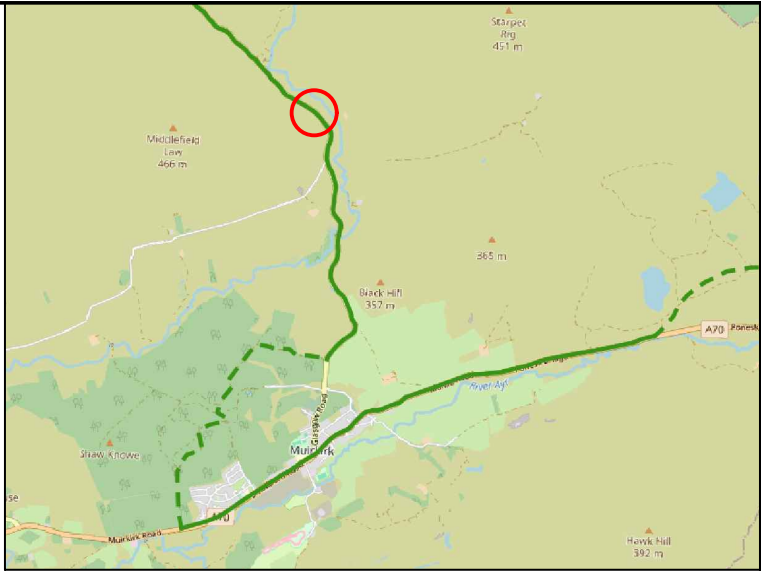
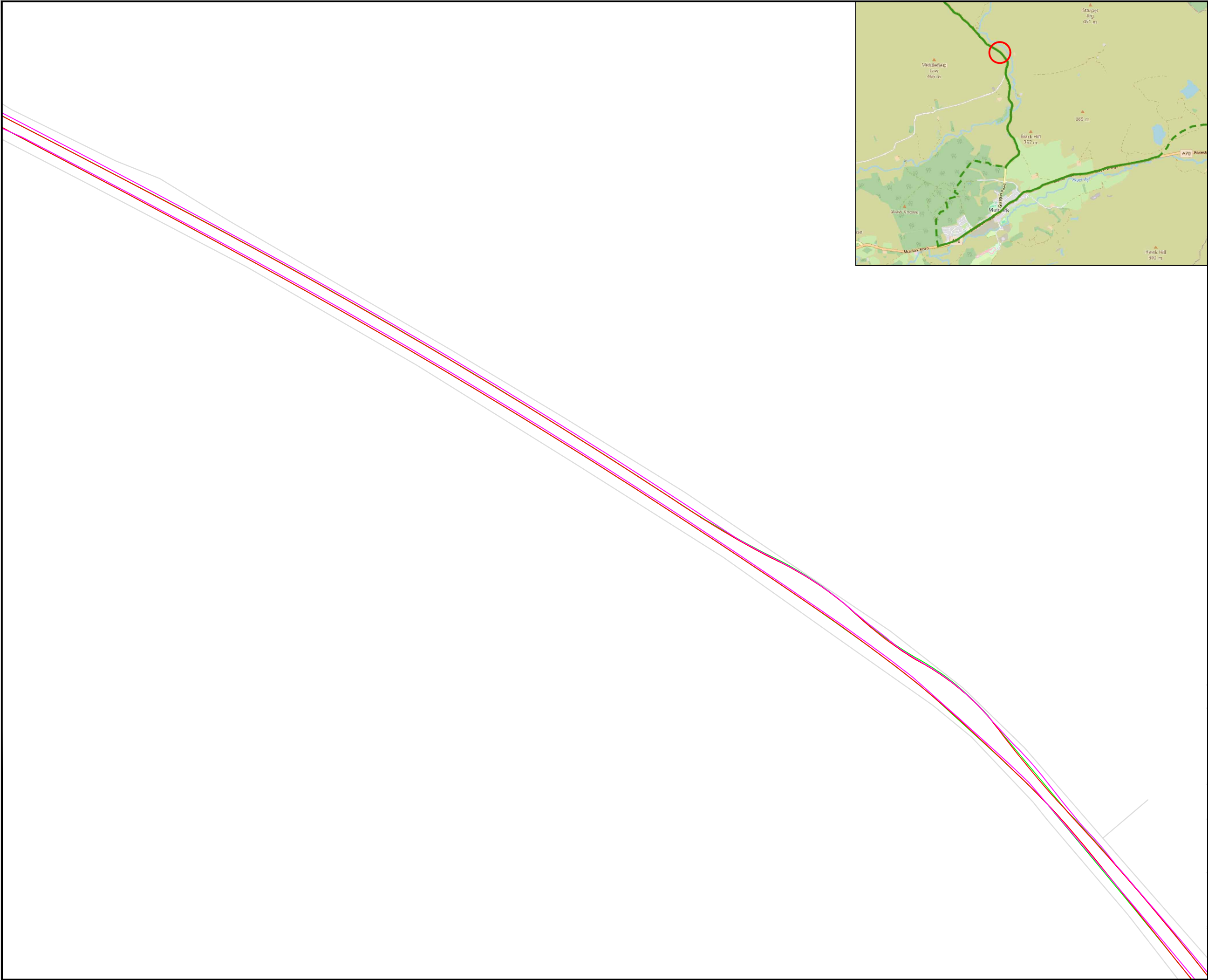
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 24

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA024	Rev.

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

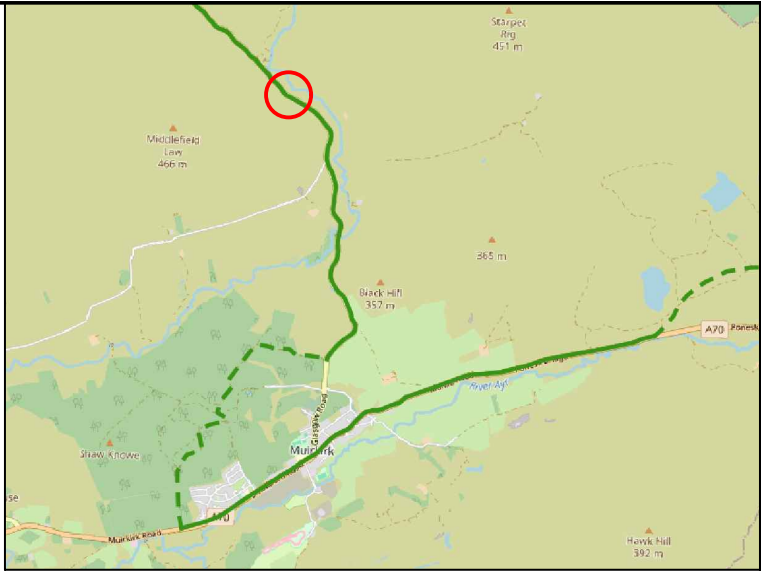
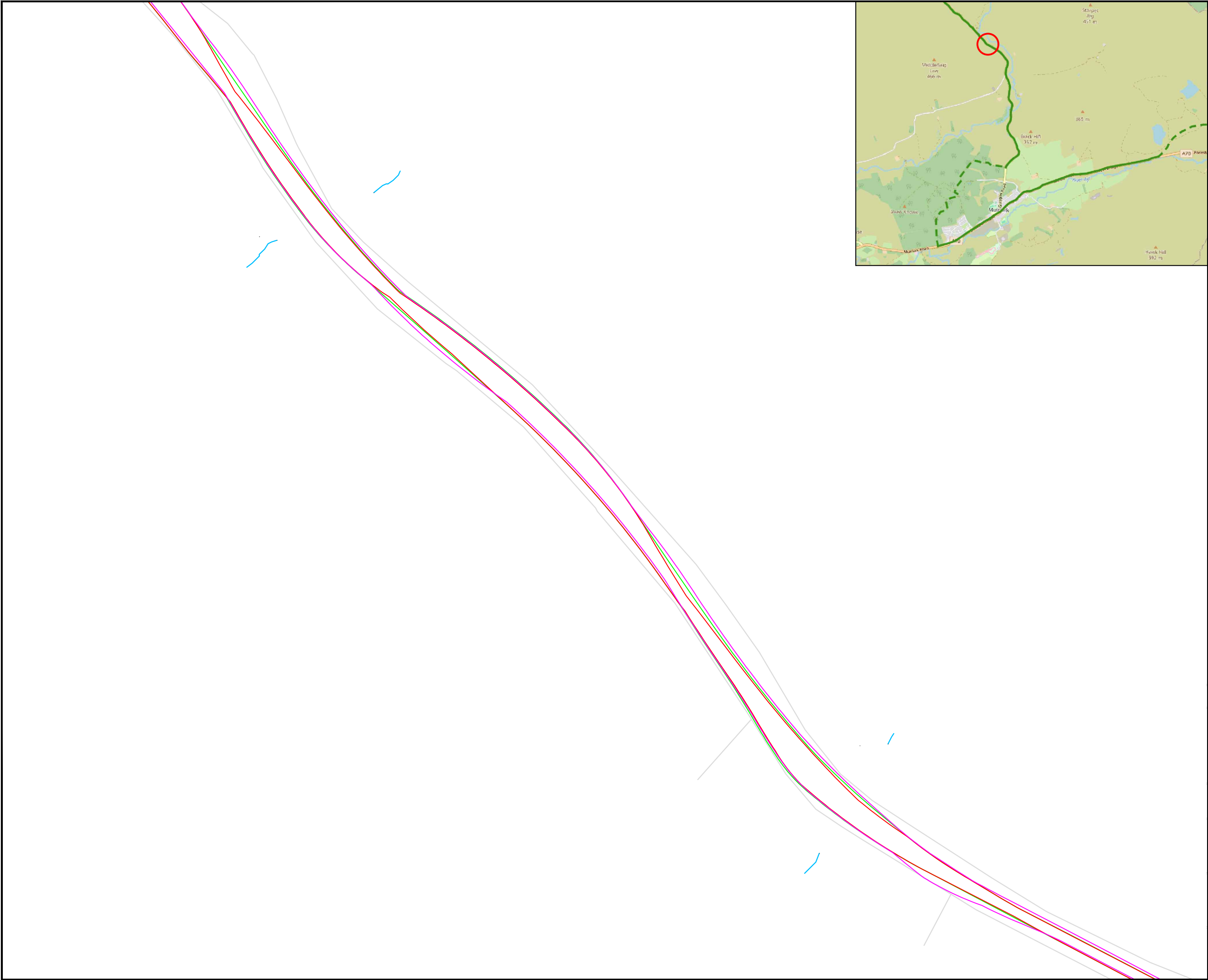
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 25

Drawn	Checked	Approved
TF	ADV	ADV


Original dtp. size	Date	Scale
A3	17/03/2025	1:1000

Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA025	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.




Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

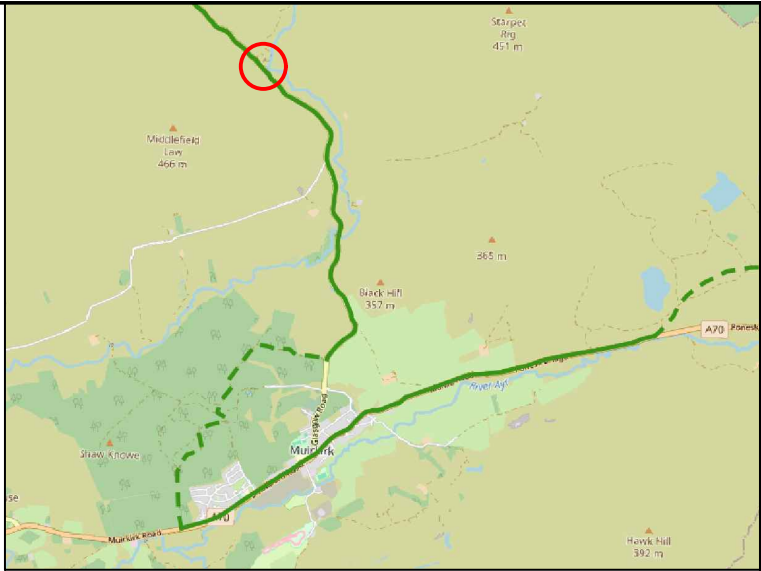
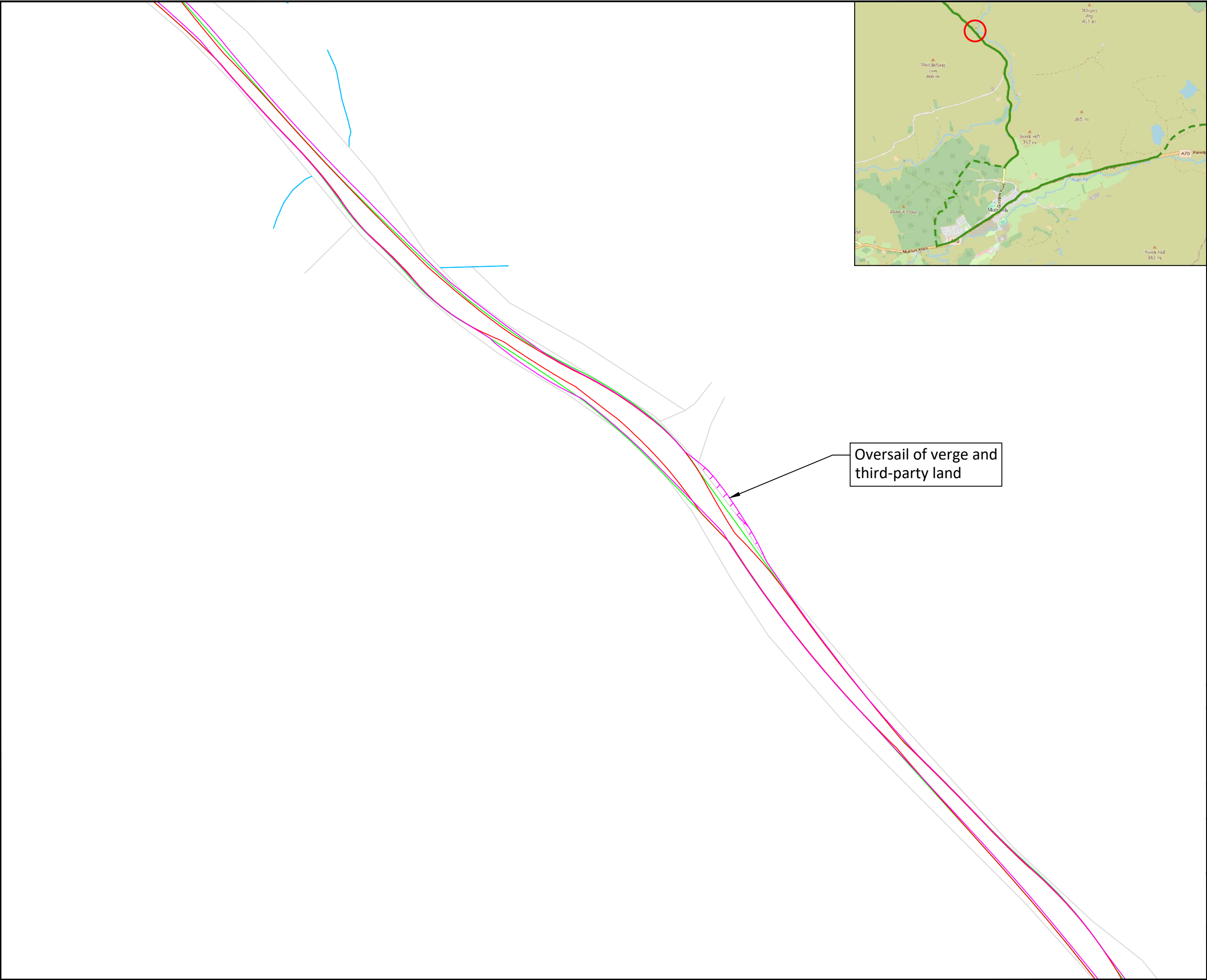
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 26

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA026	Rev.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

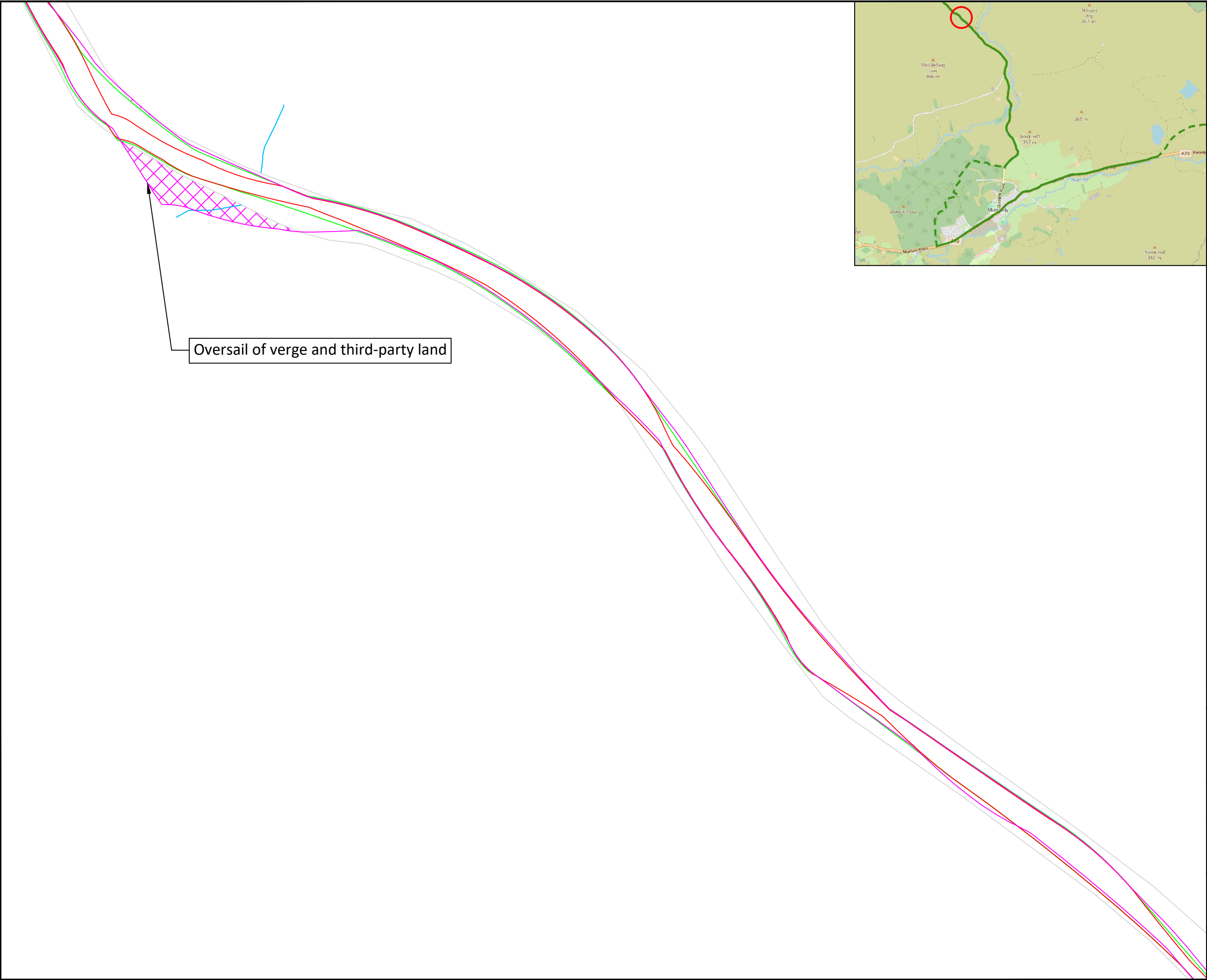
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 27

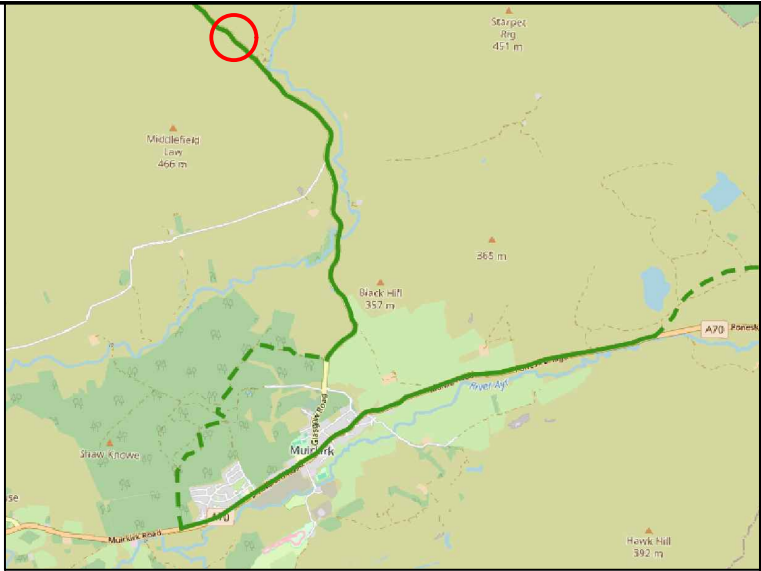
Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information		Drawing Number	GB01T24G63/2001/I/S/SPA027	Rev.

Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Oversail of verge and third-party land



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA

www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

Hagshaw Energy Cluster
Western Expansion

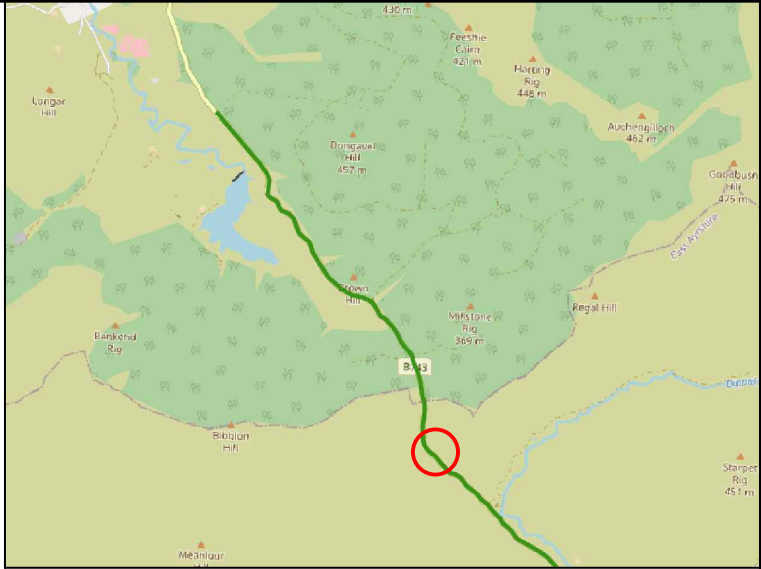
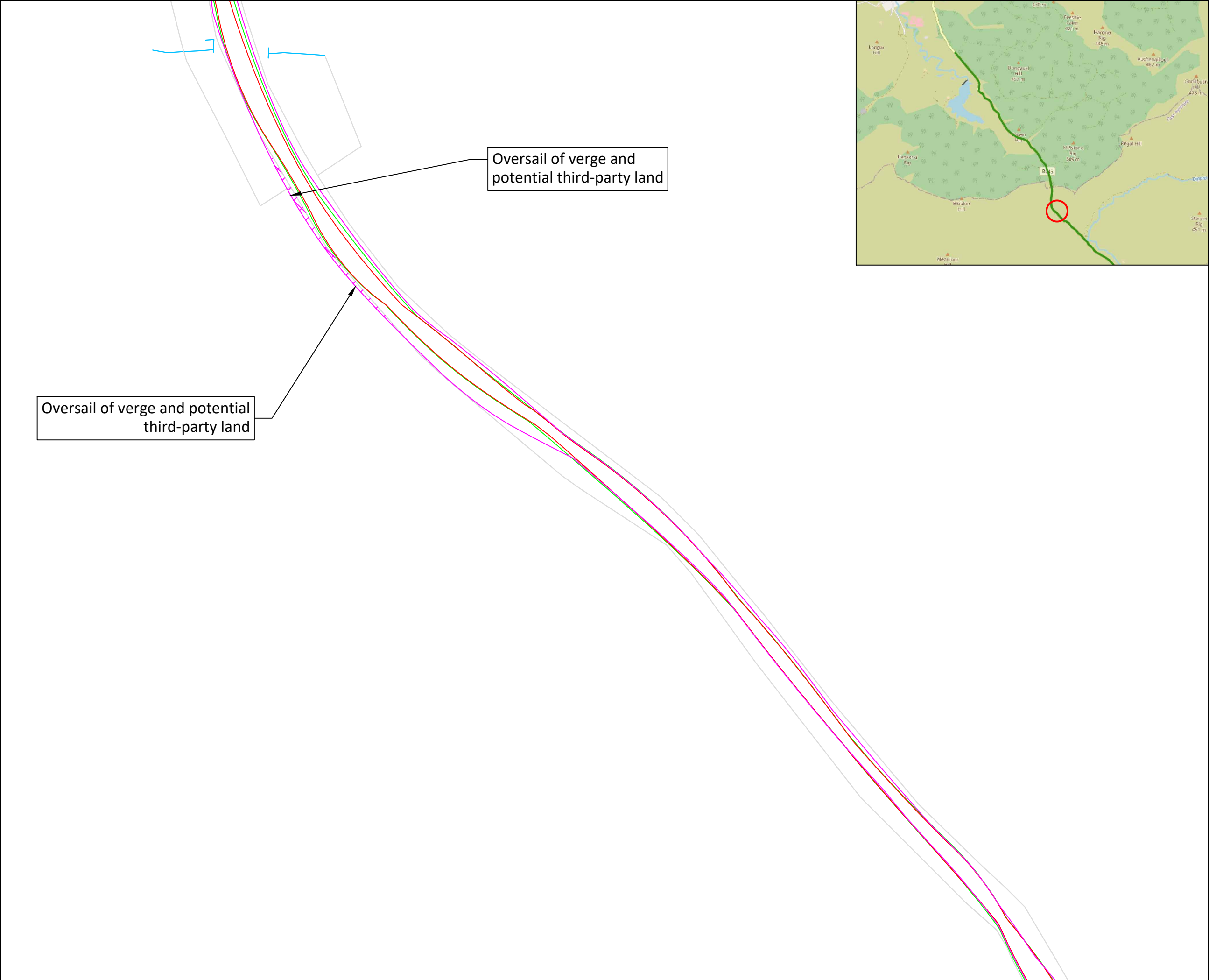
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 28

Drawn	TF	Checked	ADV	Approved	ADV
Original dwg. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status		Drawing Number		Rev.	
Information		GB01T24G63/2001/I/S/SPA028		-	


Contains OS data © Crown copyright [and database rights] [2025]

© This drawing and its content are the copyright of SYSTRA Ltd and may not be reproduced or amended except by prior written permission. SYSTRA accepts no liability for any amendments made by other persons.



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.




Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

Project

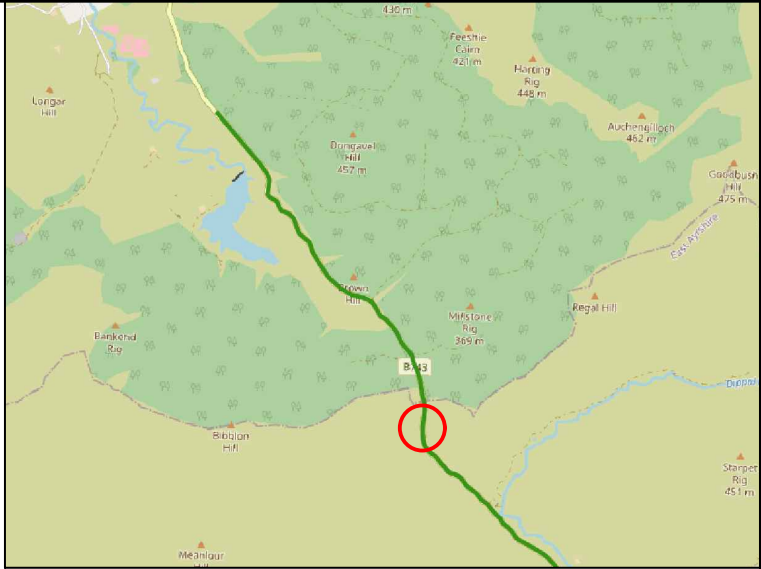
Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 29

Drawn	Checked	Approved
TF	ADV	ADV
Original dtd. size	Date	Scale
A3	17/03/2025	1:1000

Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA029	-




- Notes:**
1. Do not scale from drawing.
 2. All dimensions in metres unless otherwise stated.
 3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.

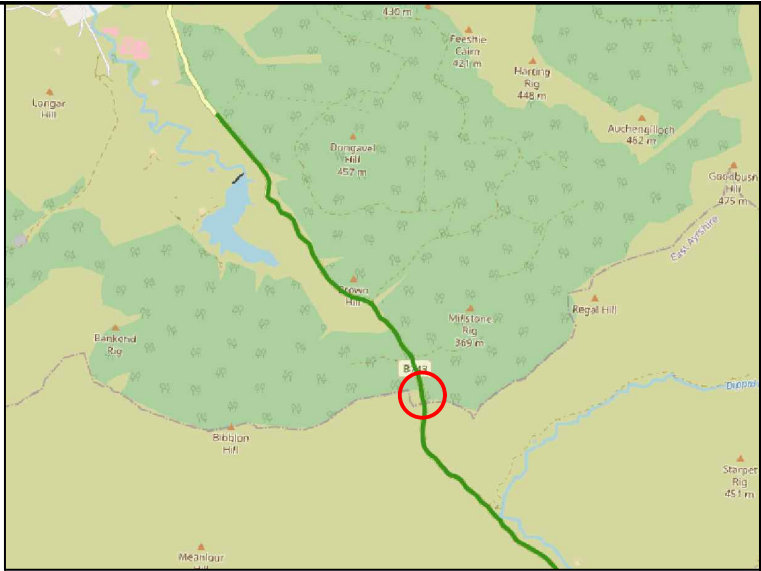


- Key:**
- Public Highway
 - Off-road Private Access Tracks
 - Blade Transporter Vehicle
 - Blade Outline
 - Over-run envelope of tractor and trailer wheels
 - Over-sail envelope being swept by tractor and trailer bed
 - Over-sail envelope being swept by blade path
 - Over-run area of tractor and trailer wheels out with carriageway
 - Over-sail area of tractor and trailer bed out with carriageway
 - Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

 www.systra.com/uk			
19 Charlotte Square, Edinburgh, EH2 4DF		T 0131 460 1847 E scotland@systra.co.uk	
Client 3R Energy			
Project Hagshaw Energy Cluster Western Expansion			
Title Swept Path Assessment 80.5m Blade "Super Wing" Blade Transport Pinch Point 30			
Drawn TF	Checked ADV	Approved ADV	
Original dtd. size A3	Date 17/03/2025	Scale 1:1000	
Drawing Status Information		Drawing Number GB01T24G63/2001/I/S/SPA030	Rev. -



- Notes:
1. Do not scale from drawing.
 2. All dimensions in metres unless otherwise stated.
 3. Please note that marked service locations are indicative only and should be confirmed by topographical survey.



- Key:
- Public Highway
 - Off-road Private Access Tracks
 - Blade Transporter Vehicle
 - Blade Outline
 - Over-run envelope of tractor and trailer wheels
 - Over-sail envelope being swept by tractor and trailer bed
 - Over-sail envelope being swept by blade path
 - Over-run area of tractor and trailer wheels out with carriageway
 - Over-sail area of tractor and trailer bed out with carriageway
 - Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.



www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

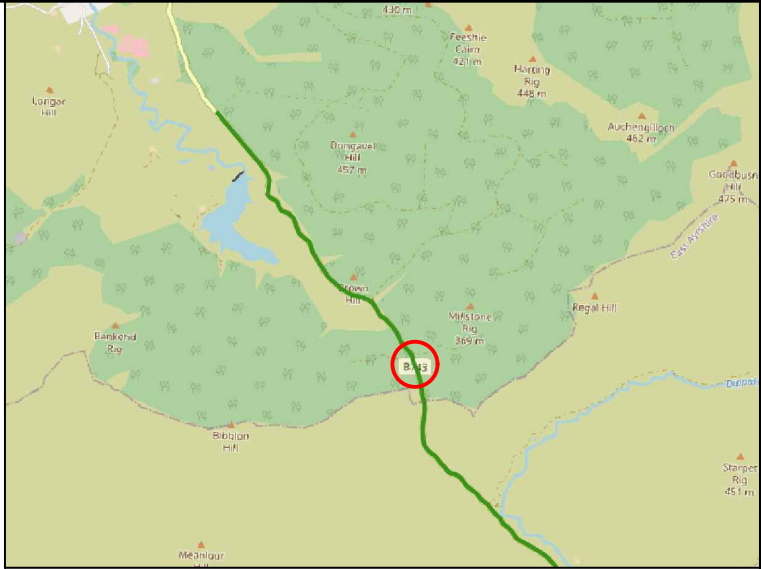
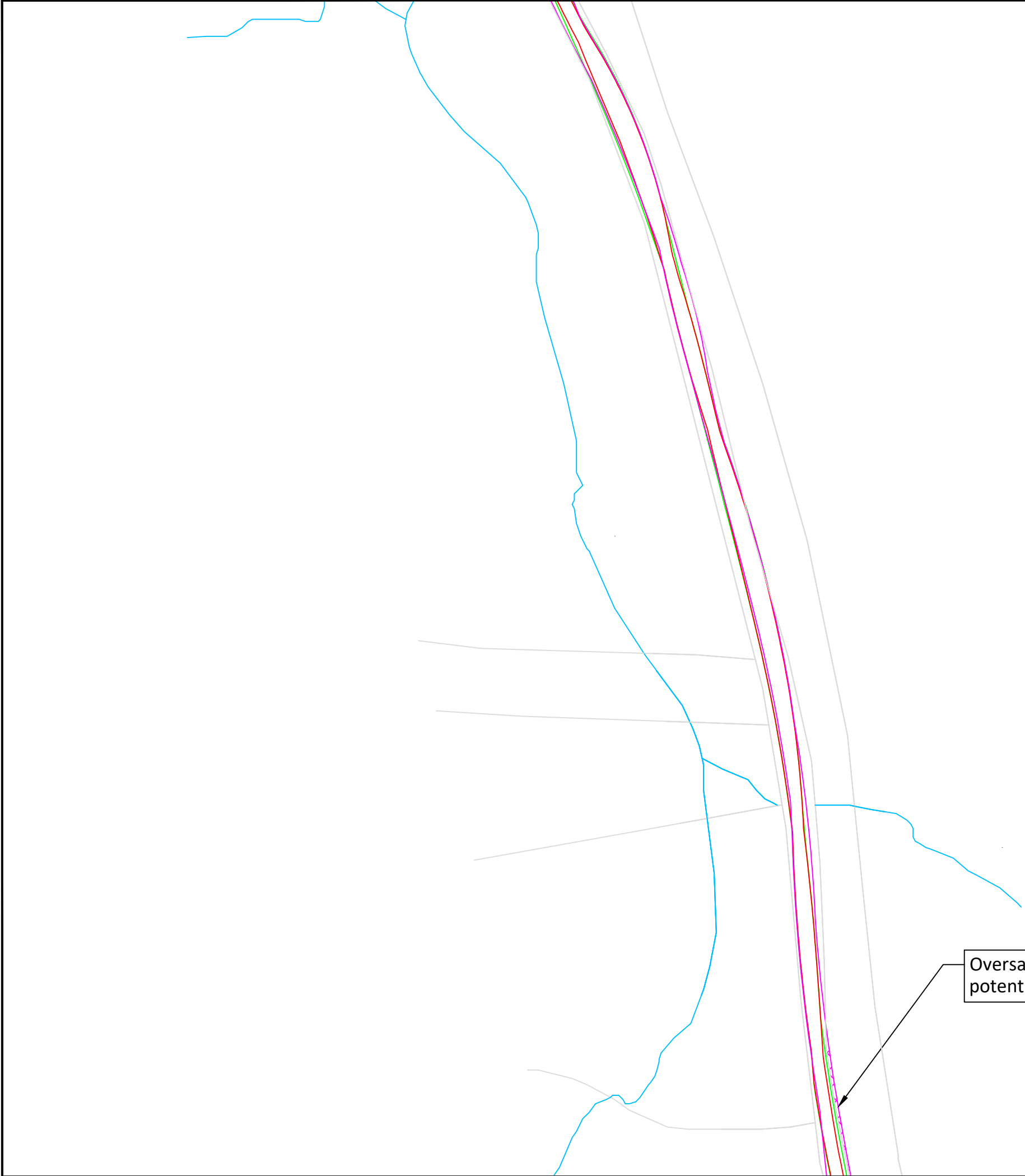
Project

Hagshaw Energy Cluster
Western Expansion

Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 31

Drawn	TF	Checked	ADV	Approved	ADV
Original dtd. size	A3	Date	17/03/2025	Scale	1:1000
Drawing Status	Information	Drawing Number	GB01T24G63/2001/I/S/SPA031	Rev.	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

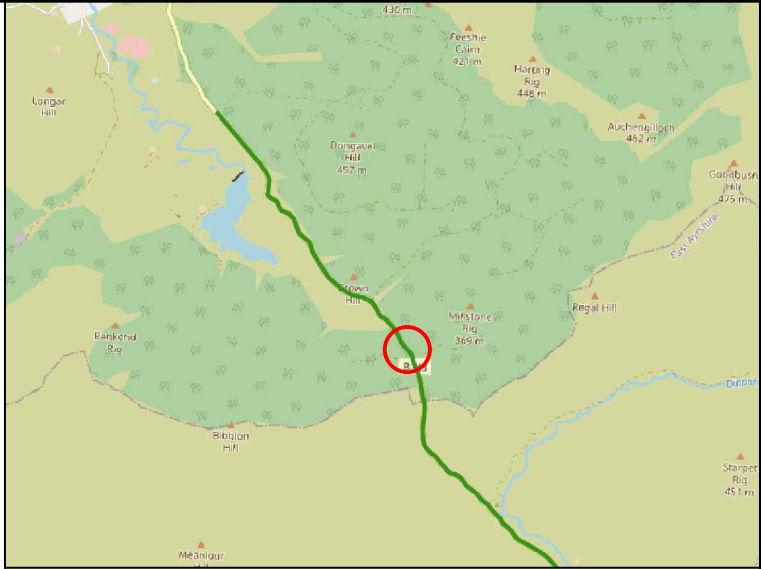
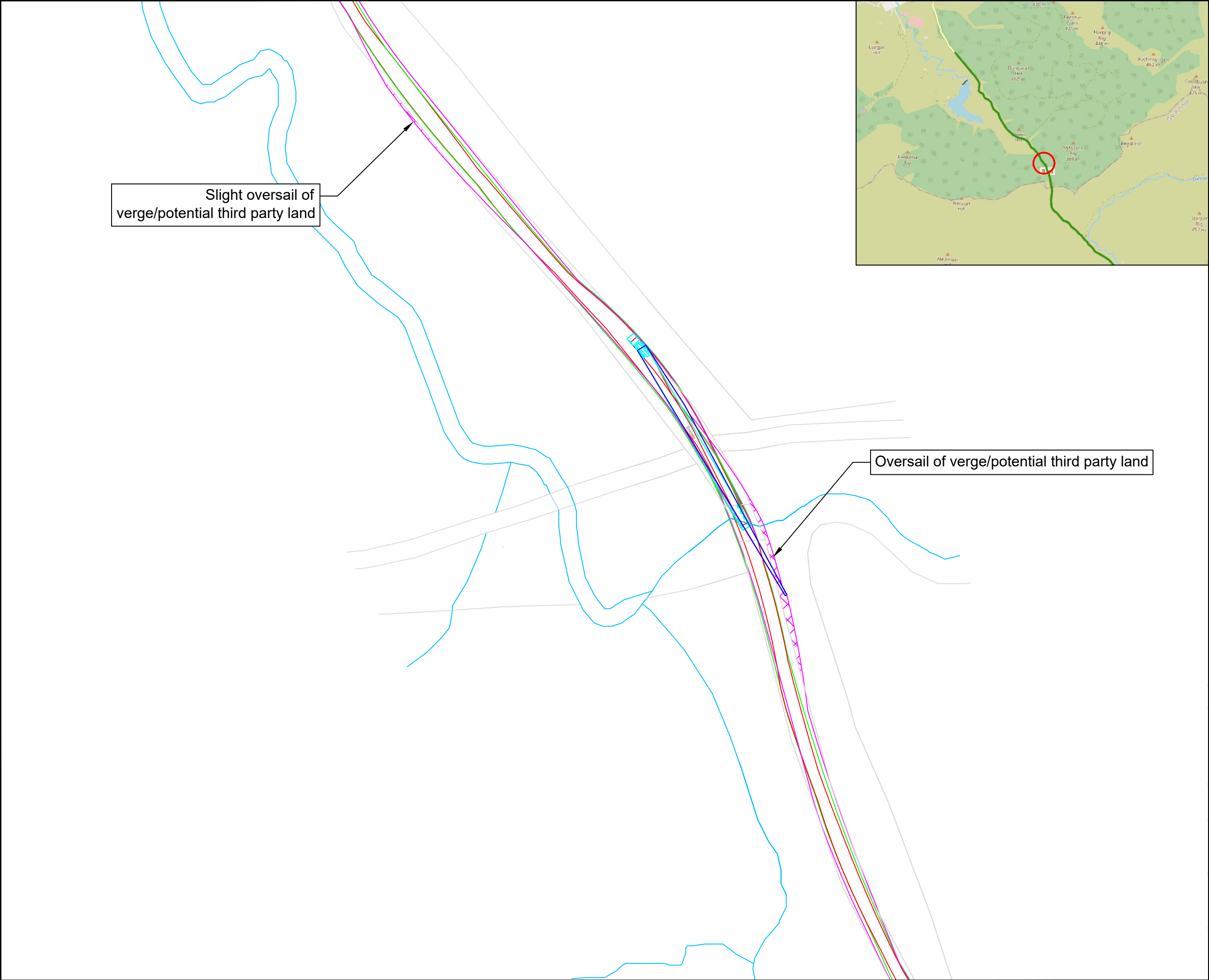
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 32

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA032

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

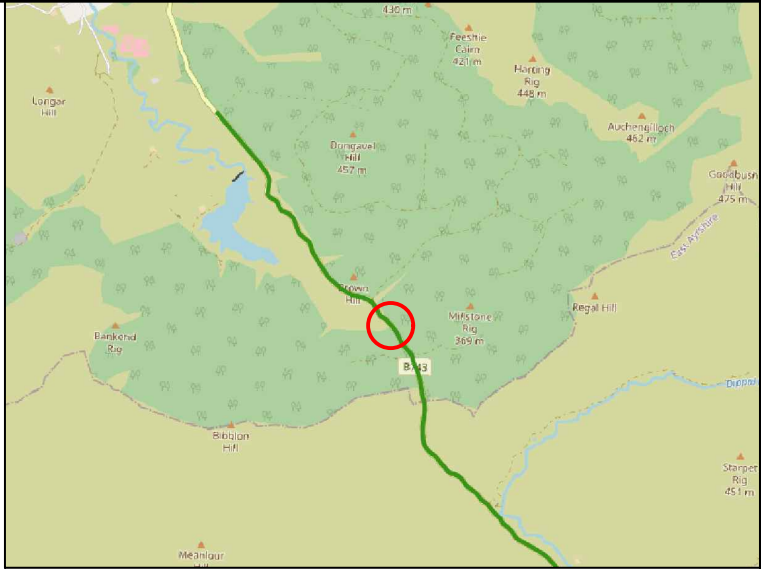
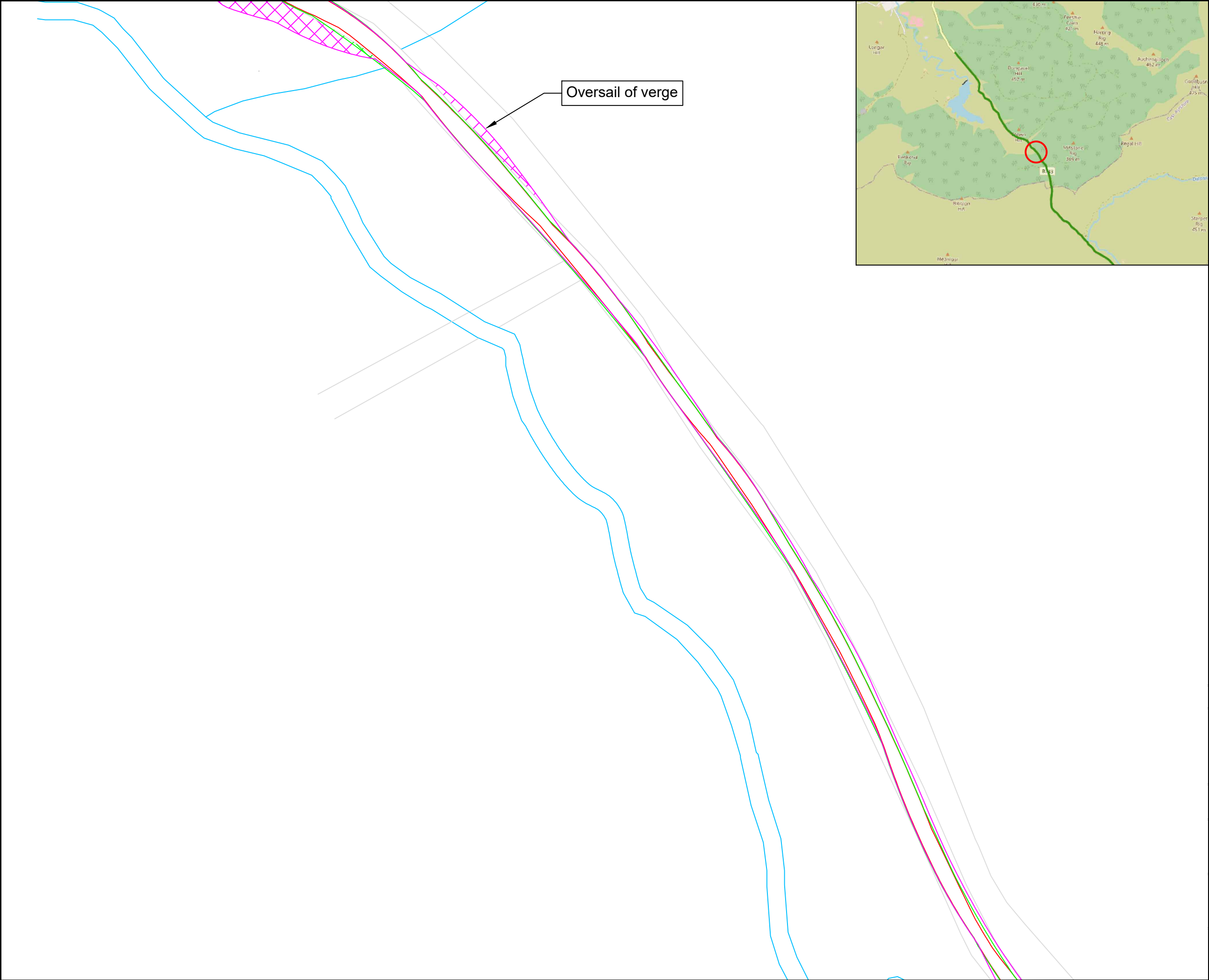
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 33

Drawn TF	Checked ADV	Approved ADV
Original dtp. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA033

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

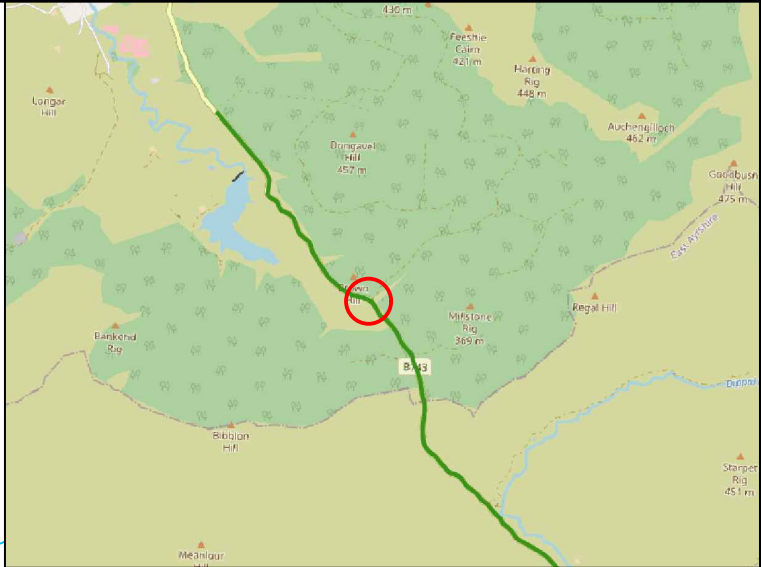
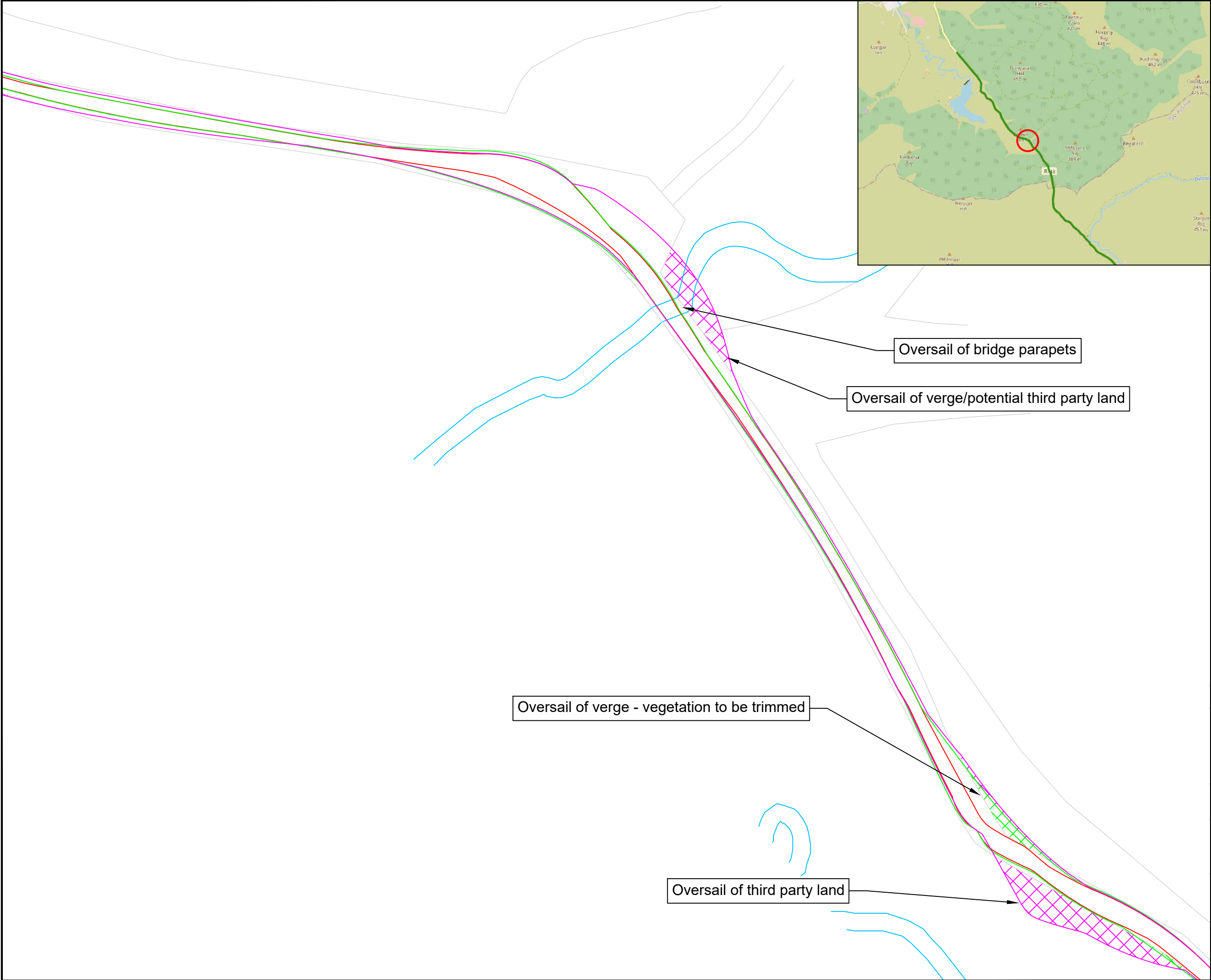
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 34

Drawn TF	Checked ADV	Approved ADV
Original dtd. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA034

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

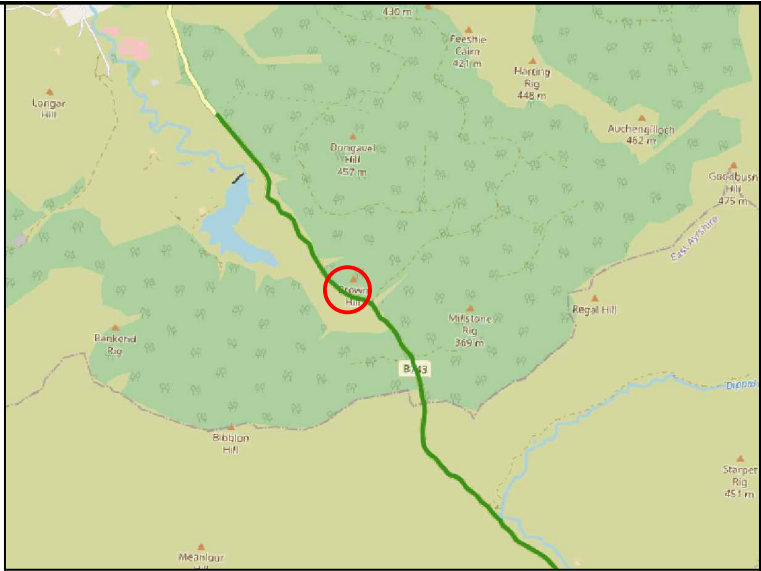
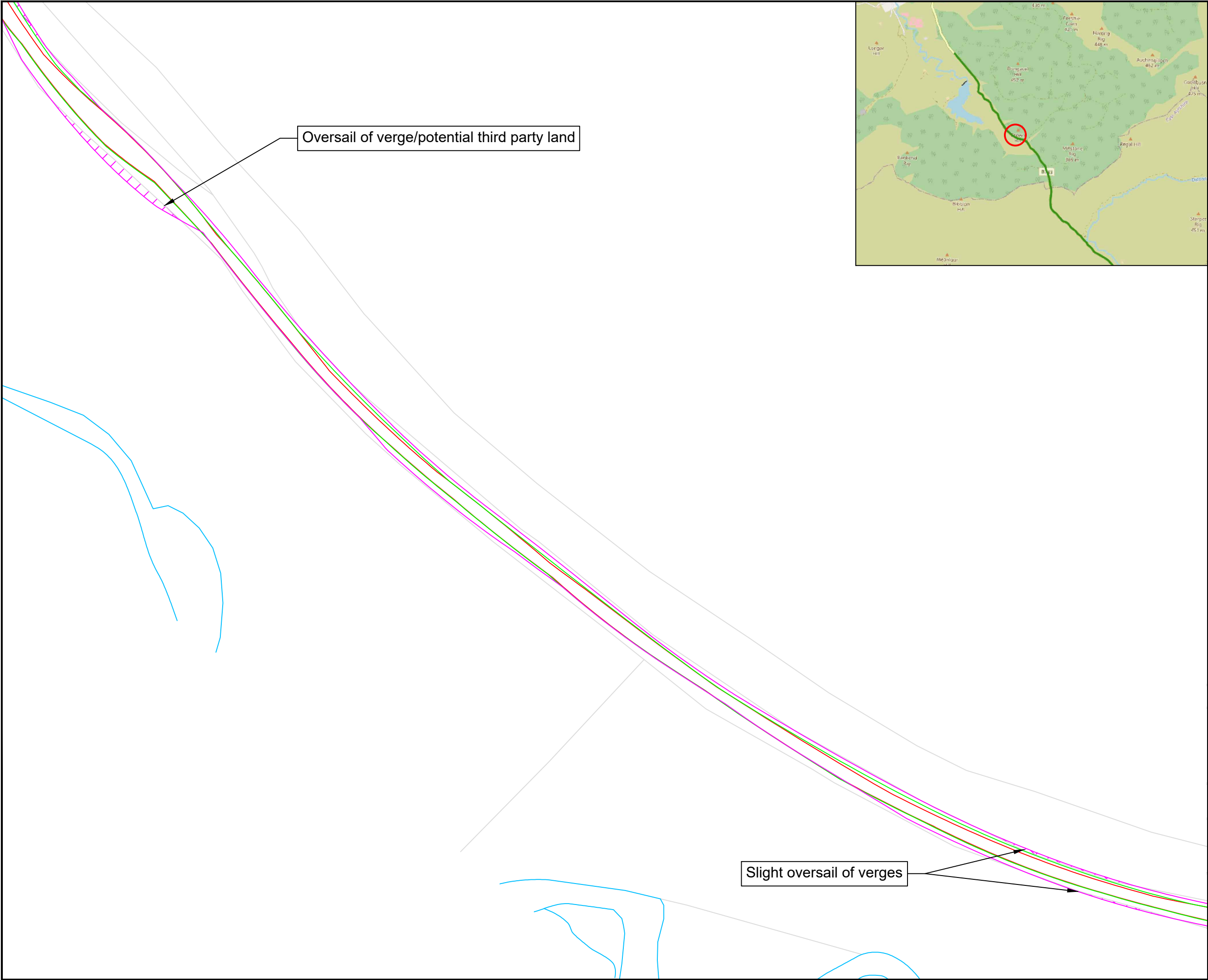
Client
3R Energy

Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 35

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000

Drawing Status Information	Drawing Number GB01T24G63/2001/I/S/SPA035	Rev. -
-------------------------------	--	-----------



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

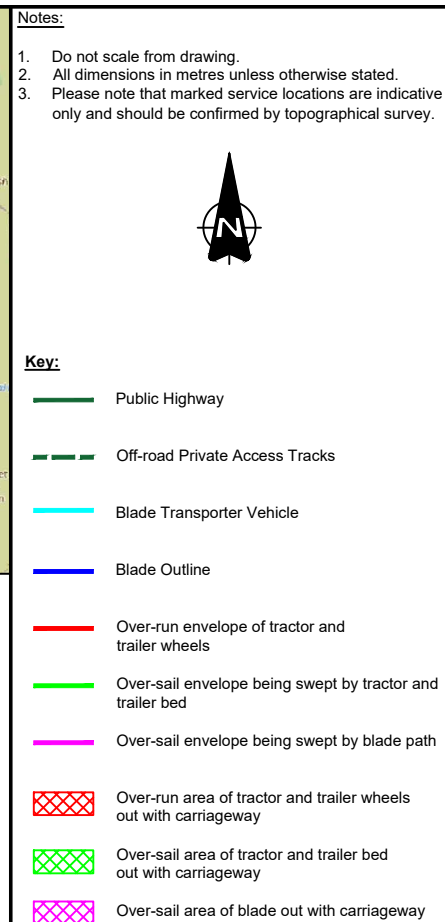
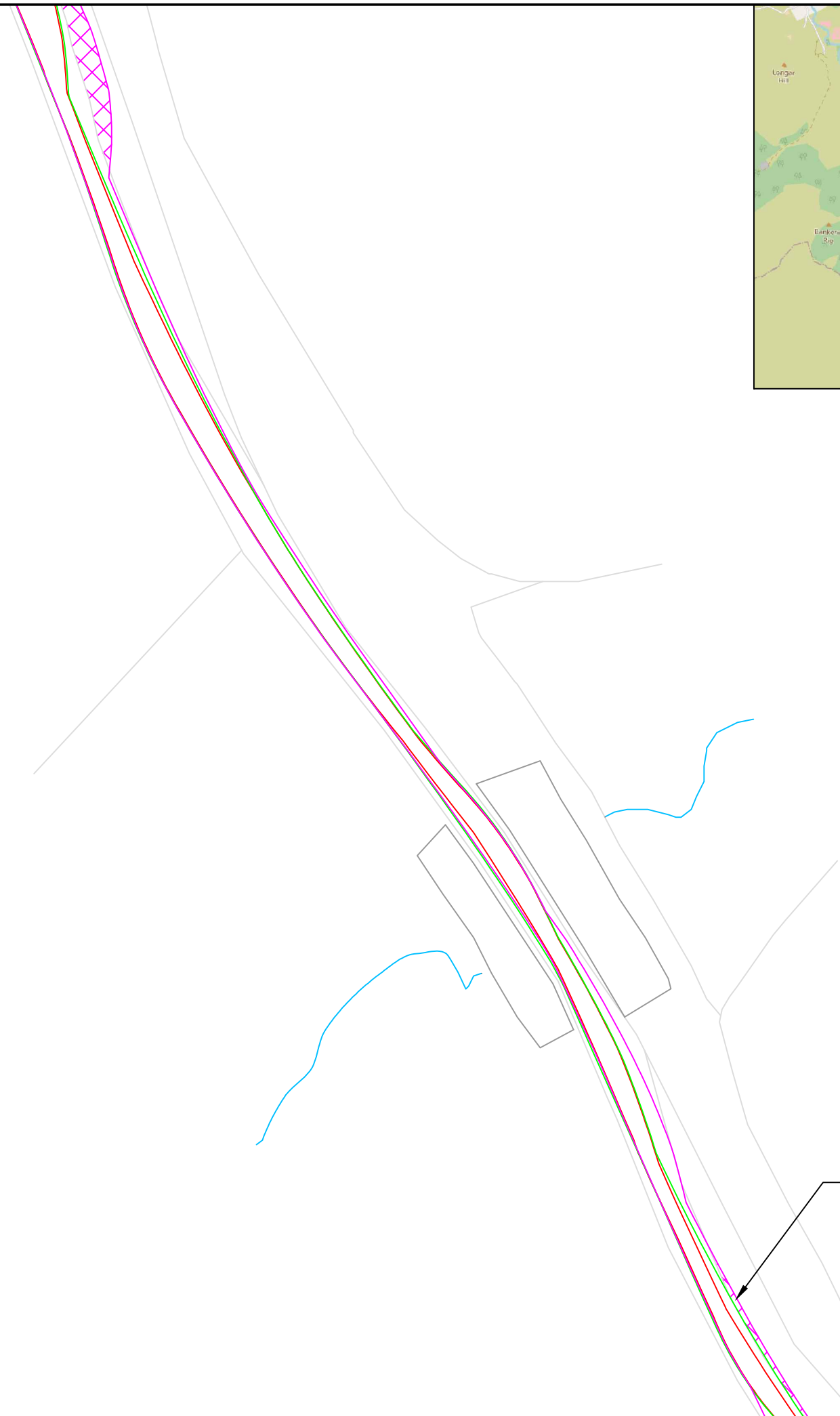
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 36

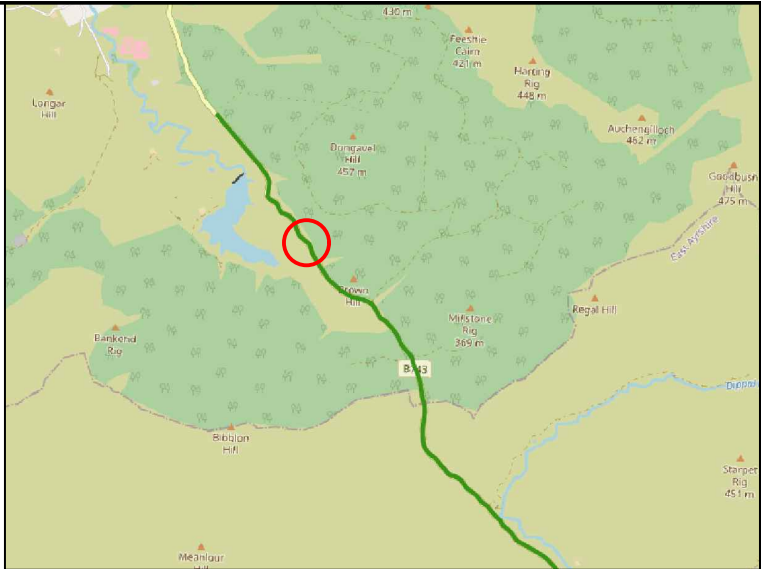
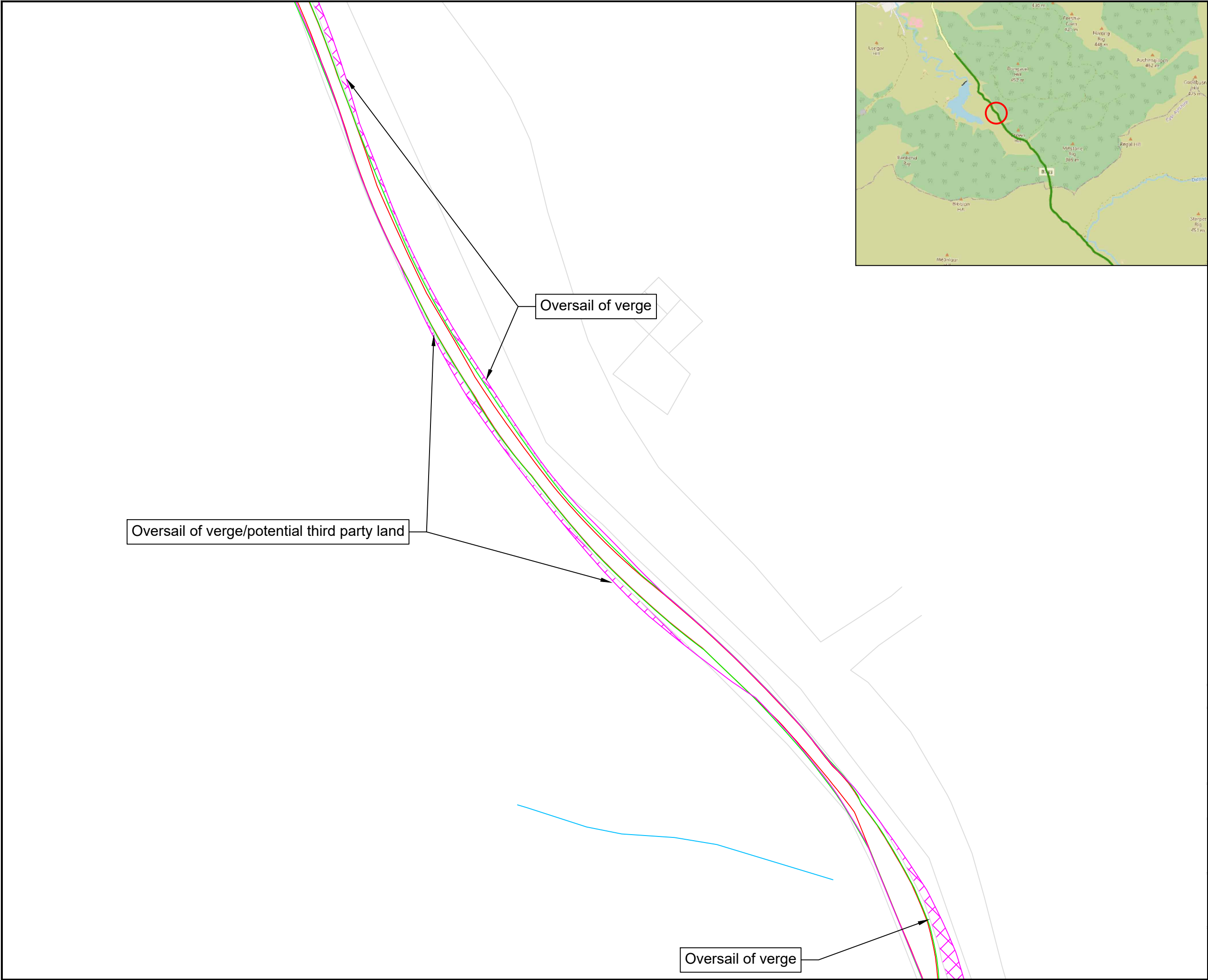
Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA036

Rev.
-



Drawn	Checked	Approved
TF	ADV	ADV
Original dwg. size	Date	Scale
A3	17/03/2025	1:1000
Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/IS/SPA037	-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

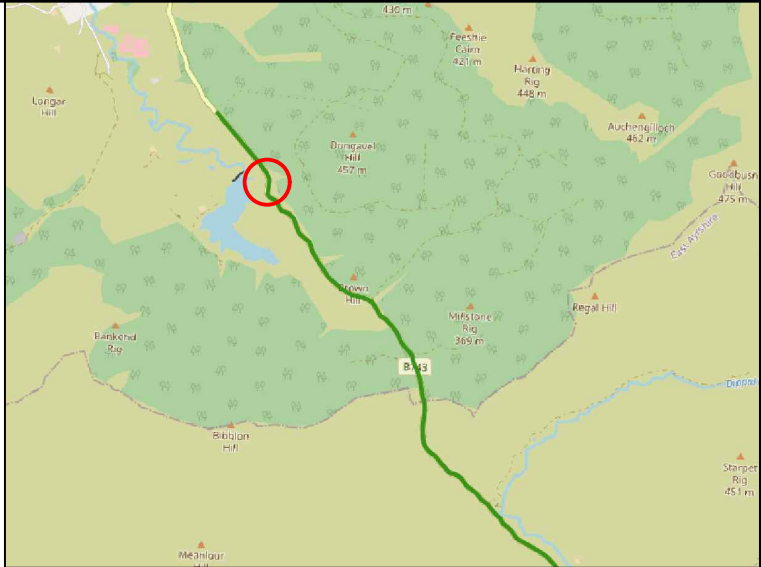
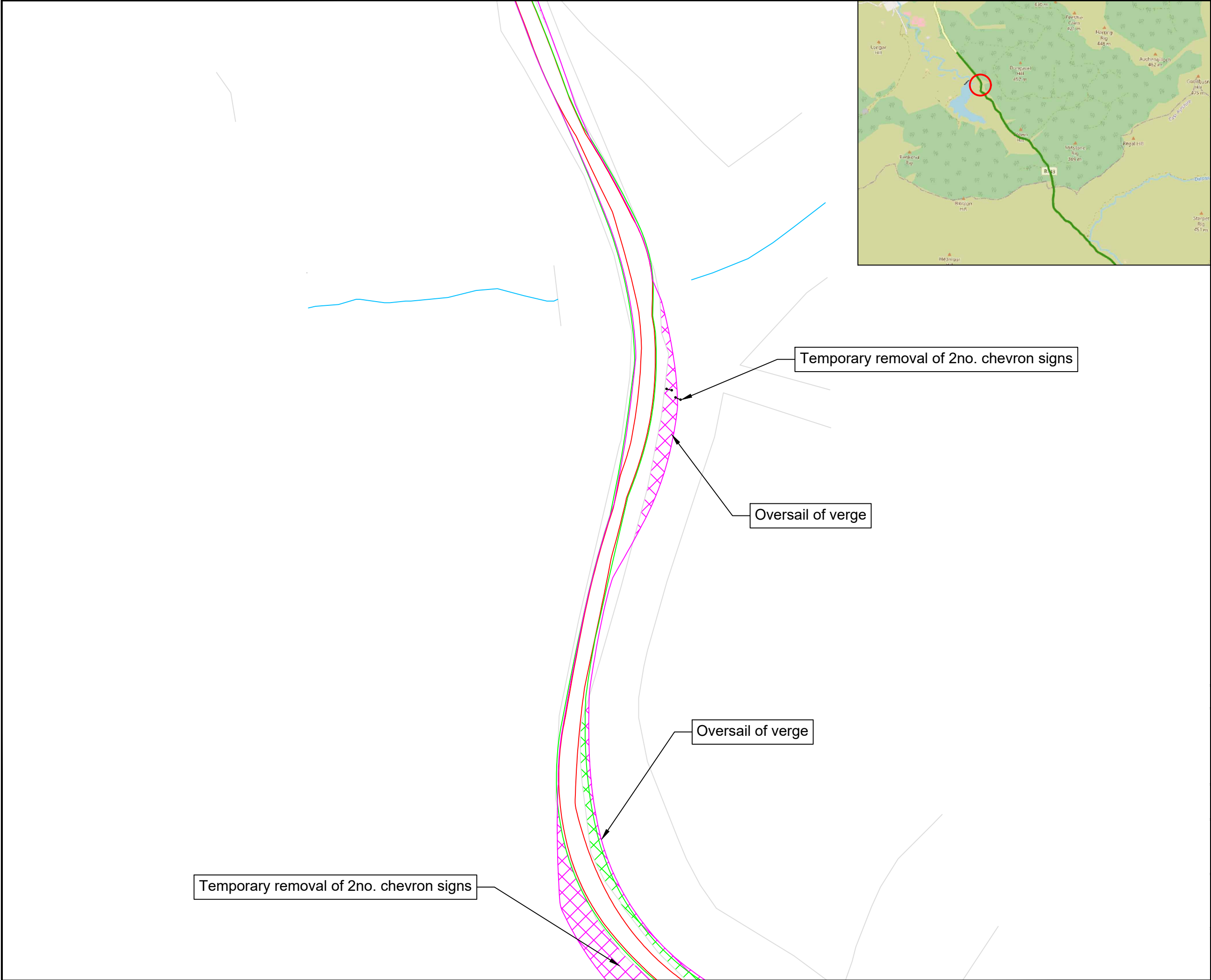
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 38

Drawn TF	Checked ADV	Approved ADV
Original dwg. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA038

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

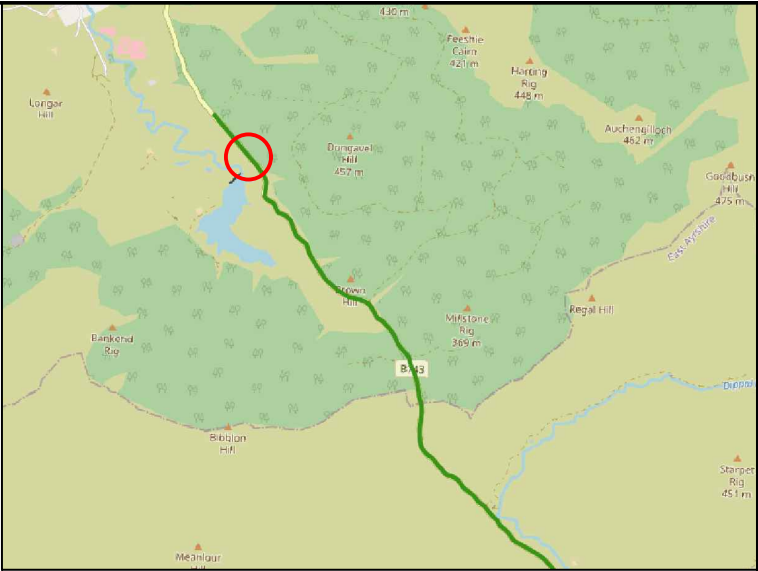
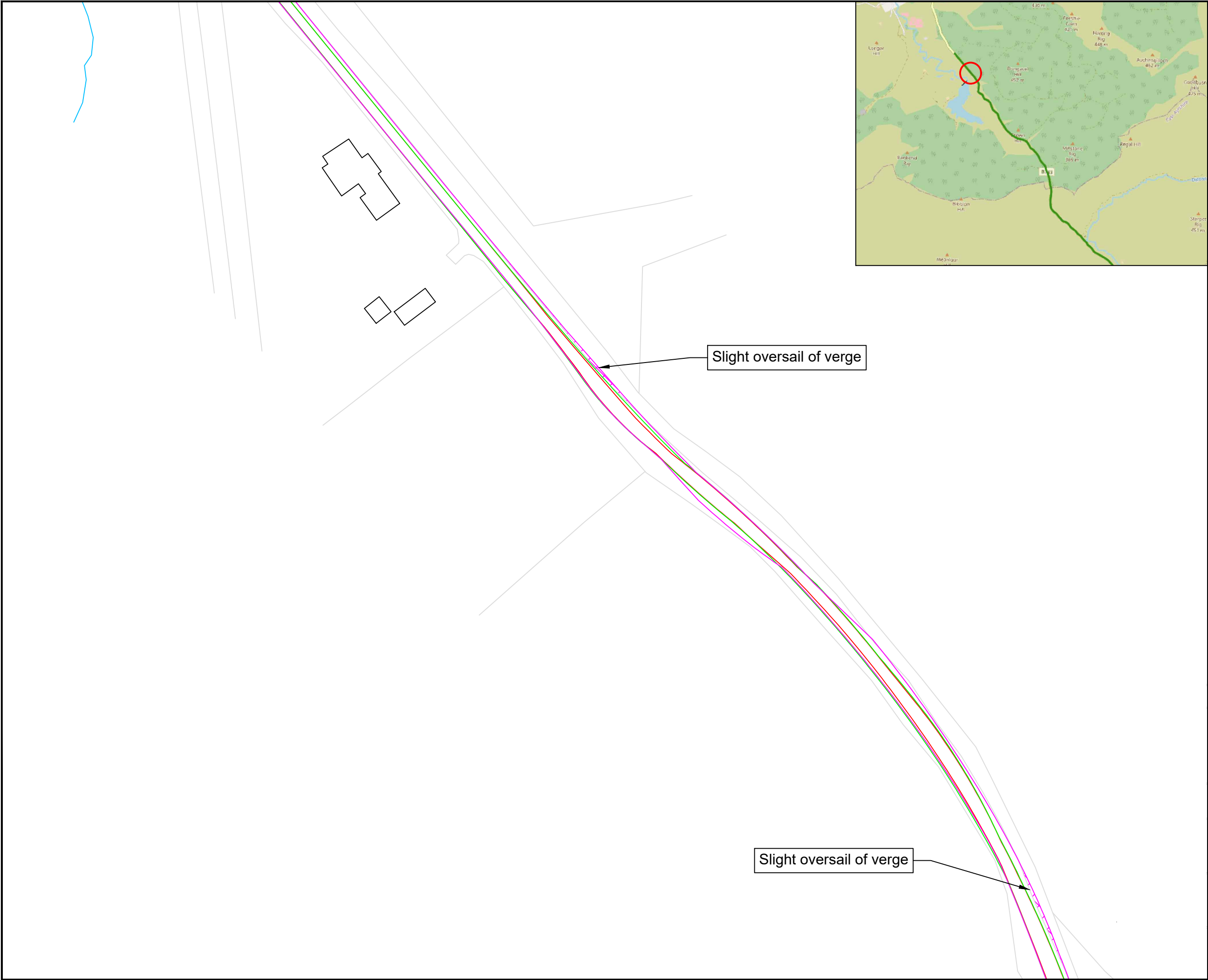
Client
3R Energy

Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 40

Drawn TF	Checked ADV	Approved ADV
Original dtd. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA040



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

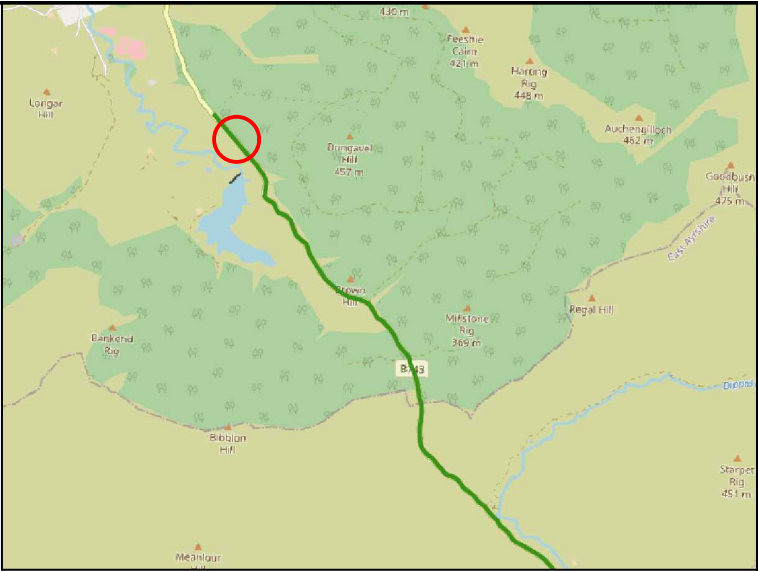
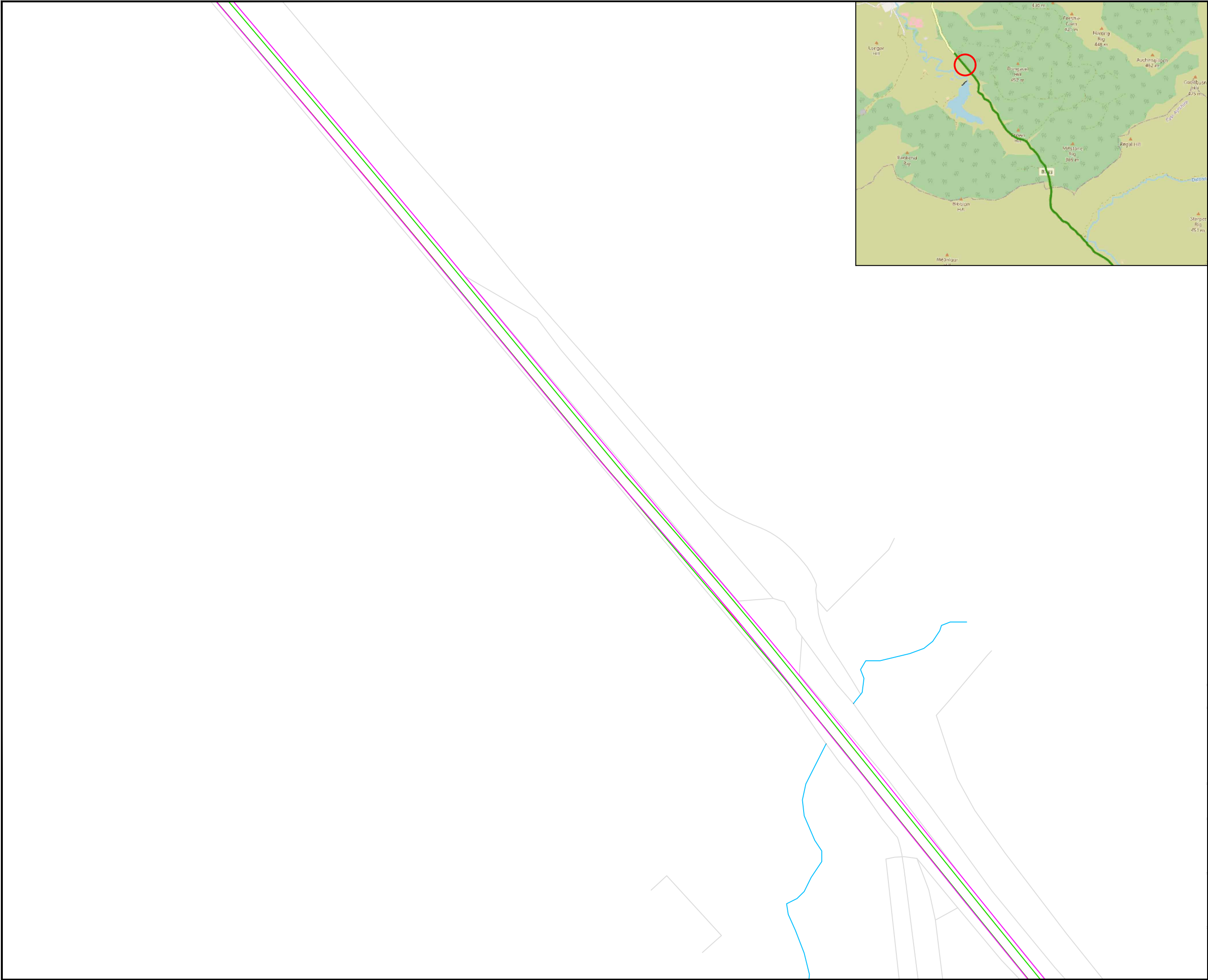
Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 41

Drawn TF	Checked ADV	Approved ADV
Original dtd. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA041

Rev.
-



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client

3R Energy

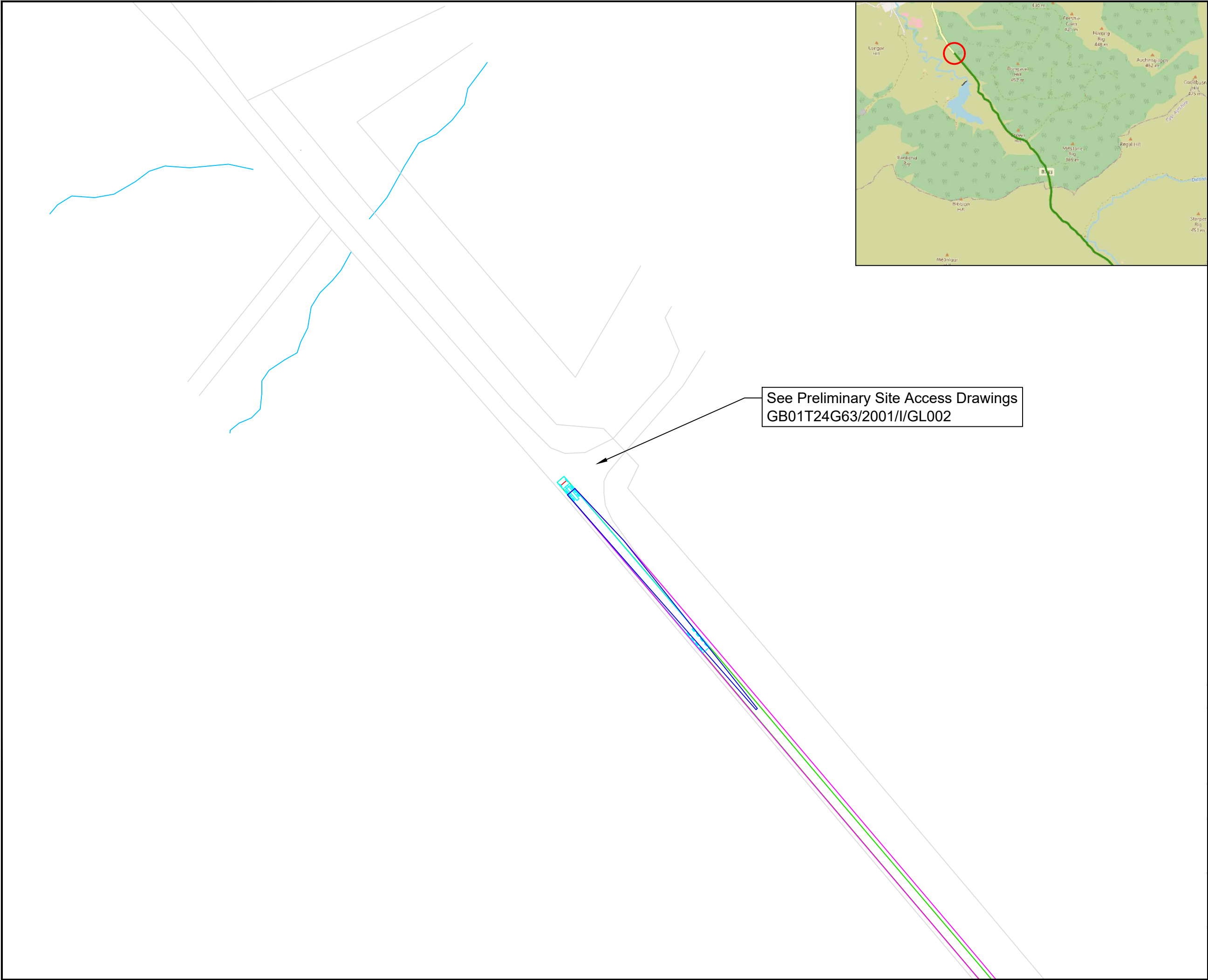
Project

Hagshaw Energy Cluster
Western Expansion

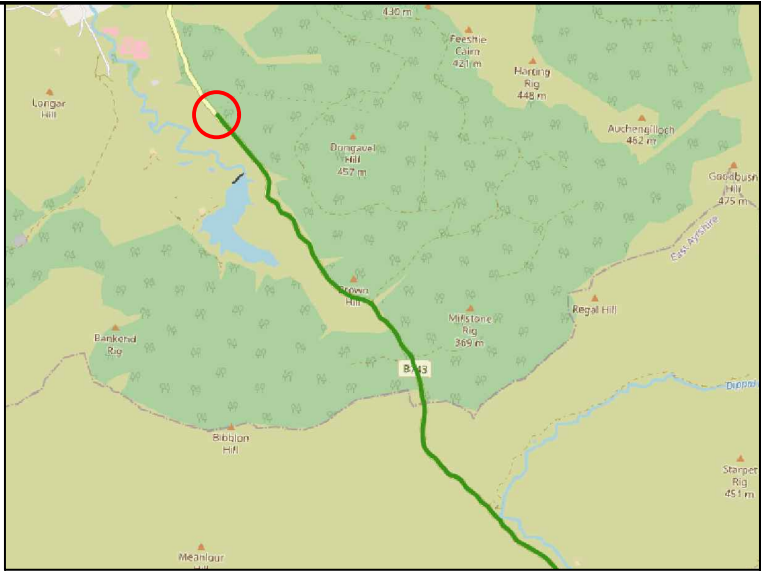
Title

Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 42

Drawn	Checked	Approved
TF	ADV	ADV
Original dtd. size	Date	Scale
A3	17/03/2025	1:1000
Drawing Status	Drawing Number	Rev.
Information	GB01T24G63/2001/I/S/SPA042	-



See Preliminary Site Access Drawings
GB01T24G63/2001/I/GL002



Notes:

- Do not scale from drawing.
- All dimensions in metres unless otherwise stated.
- Please note that marked service locations are indicative only and should be confirmed by topographical survey.

Key:

- Public Highway
- Off-road Private Access Tracks
- Blade Transporter Vehicle
- Blade Outline
- Over-run envelope of tractor and trailer wheels
- Over-sail envelope being swept by tractor and trailer bed
- Over-sail envelope being swept by blade path
- Over-run area of tractor and trailer wheels out with carriageway
- Over-sail area of tractor and trailer bed out with carriageway
- Over-sail area of blade out with carriageway

Rev.	Date	Revision details	Drawn	Checked	Approved

© This drawing is the property of SYSTRA Limited and the information can only be reproduced with their prior permission.

SYSTRA
www.systra.com/uk

19 Charlotte Square,
Edinburgh,
EH2 4DF

T 0131 460 1847
E scotland@systra.co.uk

Client
3R Energy

Project
Hagshaw Energy Cluster
Western Expansion

Title
Swept Path Assessment
80.5m Blade "Super Wing" Blade Transport
Pinch Point 43

Drawn TF	Checked ADV	Approved ADV
Original dtd. size A3	Date 17/03/2025	Scale 1:1000
Drawing Status Information		Rev. -

Drawing Number
GB01T24G63/2001/I/S/SPA043

Rev.
-

SYSTRA provides advice on transport, to central, regional and local government, agencies, developers, operators and financiers.

A diverse group of results-oriented people, we are part of a strong team of professionals worldwide. Through client business planning, customer research and strategy development we create solutions that work for real people in the real world.

For more information visit www.systra.com/uk

Birmingham

Alpha Tower, Crowne Plaza, Suffolk Street
Birmingham, B1 1TT
T: +44 (0)121 393 4841

Bristol

33 Colston Avenue, Bristol, BS1 4UA

Cork

City Quarter, Lapps Quay, Cork City
Cork, T12 WY42, Republic of Ireland

Dublin

2nd Floor, Riverview House, 21-23 City Quay
Dublin D02 AY91, Republic of Ireland
T: +353 (0) 1 566 2028

Edinburgh

Ground Floor, 18 Charlotte Square, Edinburgh, EH2 4DF
T: +44 (0)131 460 1847

Glasgow

The Centrum Business Centre Limited, 38 Queen Street, Glasgow,
G1 3DX
T: +44 (0)141 468 4205

Leeds

100 Wellington Street, Leeds, LS1 1BA
T: +44 (0)113 360 4842

London

One Carey Lane, London, England EC2V 8AE
T: +44 (0)20 3855 0079

Manchester

5th Floor, Four Hardman Street, Spinningfields
Manchester, M3 3HF
Tel: +44 (0)161 504 5026

Newcastle

Block C, First Floor, Portland House, New Bridge Street West,
Newcastle, NE1 8AL
Tel: +44 191 249 3816

Reading

Davidson House, Forbury Square,
Reading, RG1 3EU
T: +44 118 208 0111

Woking

Dukes Court, Duke Street
Woking, Surrey GU21 5BH
T: +44 (0)1483 357705

York

Meridian House, The Crescent
York, YO24 1AW
Tel: +44 1904 454 600

Other locations:

France:

Bordeaux, Lille, Lyon, Marseille, Paris

Northern Europe:

Astana, Copenhagen, Kiev, London, Moscow, Riga, Wroclaw

Southern Europe & Mediterranean: Algiers, Baku, Bucharest,

Madrid, Rabat, Rome, Sofia, Tunis

Middle East:

Cairo, Dubai, Riyadh

Asia Pacific:

Bangkok, Beijing, Brisbane, Delhi, Hanoi, Hong Kong, Manila,
Seoul, Shanghai, Singapore, Shenzhen, Taipei

Africa:

Abidjan, Douala, Johannesburg, Kinshasa, Libreville, Nairobi

Latin America:

Lima, Mexico, Rio de Janeiro, Santiago, São Paulo

North America:

Little Falls, Los Angeles, Montreal, New-York, Philadelphia,
Washington

SYSTRA